

Statement of Response

Proposed Strategic
Housing Development at
Bóthar an Chóiste,
Castlegar, Galway





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Table of Contents

1.	INTRODUCTION.....	2
2.	RESPONSE TO ISSUES RAISED REGARDING SPECIFIC INFORMATION TO BE SUBMITTED.....	4
2.1	Statement of Consistency	4
2.1.1	Applicants Response	4
2.2	Material Contravention Statement.....	4
2.2.1	Applicants Response	4
2.3	Connection and Integration with Overall Area	5
2.3.1	Applicants Response	5
2.4	Traffic and Transport Infrastructure	6
2.4.1	Applicants Response	6
2.5	Car and Bicycle Parking Layout	6
2.5.1	Applicants Response	6
2.6	Residential Amenity: Overlooking & Overshadowing	7
2.6.1	Applicants Response	7
2.7	Daylight and Shadow Impact Assessment	7
2.7.1	Applicants Response	7
2.8	Response to Planning Authority Opinion.....	7
2.8.1	Applicants Response	8
2.9	Landscaping Plans	14
2.9.1	Applicants Response	15
2.10	Noise Action Plan.....	15
2.10.1	Applicants Response	15
2.11	Building Lifecycle Report.....	15
2.11.1	Applicants Response	15
2.12	Updated Ecological Impact Assessment	15
2.12.1	Applicants Response	15
2.13	Taking in Charge Plan	16
2.13.1	Applicants Response	16
2.14	Construction and Demolition Waste Management Plan.....	16
2.14.1	Applicants Response	16
3.	CONCLUSION	17

APPENDICES

Appendix 1 – Galway City Council Opinion Report

TABLE OF FIGURES

Figure 1: Fig 11.6 Lands to the North of Bóthar na Chóiste with the subject site outlined in blue.....14

1. INTRODUCTION

Following a Section 247 pre-application consultation with Galway City Council, a request to enter Stage 2 pre-planning consultations with An Bord Pleanála (ABP) was submitted to the Board on 14th December 2021. ABP convened a tripartite meeting with the prospective applicant and Galway City Council on 21st March 2022. ABP issued a Notice of Pre-Application Consultation Opinion on the submitted documentation following this meeting which stated that the Board “*is of the opinion that the documents submitted with the request to enter into consultations constitute a reasonable basis for an application for strategic housing development*”.

Fourteen specific items of information were requested to be submitted and were as follows:

1. A detailed statement of consistency and planning rationale, clearly outlining how in the prospective applicant’s opinion, the proposal is consistent with the specific objectives of the Galway City Council Development Plan 2017 – 2023, in particular with respect to the Galway City Ring Road (GCRR) reservation corridor and to the specific objective for implementation of road widening and improvements at Bothar na Choiste both to the west to the Castlegar Local Centre and east to the Tuam Road, in light of the concerns raised in the Planning Authority’s opinion.
2. A detailed statement, which should provide adequate identification of all such elements and justification as applicable, where the proposed development materially contravenes the Development Plan other than in relation to the zoning of the land, indicating why permission should, nonetheless, be granted, having regard to a consideration specified in section 37(2)(b) of the Act of 2000. In this regard, in addition to car parking contravention as identified by the applicant, consideration should also be given to possible contravention of the GCRR reservation route and plot ratio standards for new residential development as set out in the Galway City Development Plan 2017 – 2023.
3. An assessment on how the proposed scheme ties in with, connects and integrates with the expansion of the overall area. The subject site represents an expansion of the outer suburbs of Galway city centre at Castlegar. It is important that safe pedestrian and cycle connections to Galway city centre and to the development to the south is delivered. There needs to be strong permeability within the scheme and to the Castlegar local centre.
4. Further consideration of the traffic and transportation infrastructure upgrades required, who is responsible for delivery of upgrade and improvement of Bothar na Choiste, as per the specific objective set out in the CDP 2017 – 2023, and timeframe for delivery of the upgrades in tandem with delivery of residential development.
5. Further justification of quantum and layout of car parking and bicycle parking.
6. A report that addresses issues of residential amenity, specifically with regards to potential overlooking, overshadowing and overbearing. The report shall include full and complete drawings including levels and cross-sections showing the relationship between the proposed development and any adjacent existing or permitted development.
7. A Daylight and Shadow Impact Assessment of the proposed development, specifically with regard to impact upon adequate daylight and sunlight for individual units, public open space, courtyards, communal areas, private amenity spaces and balconies.
8. A full response to matters raised within the PA Opinion and addendum reports submitted to ABP on the 02.02.2022.

9. Detailed landscape drawings that illustrate hard and soft landscaping, useable communal open space, meaningful public open space, quality audit and way finding. The public open space shall be usable space, accessible and overlooked to provide a degree of natural supervision. Details of play equipment, street furniture including public lighting and boundary treatments should be submitted.
10. A Noise Action Plan and Assessment.
11. A full and detailed Building Lifecycle Report.
12. An up-to-date Ecological Impact Assessment, inclusive of a Bird and Bat Survey.
13. A site layout plan indicating what areas, if any, are to be taken in charge by the planning authority.
14. Site Specific Construction and Demolition Waste Management Plan.

2. RESPONSE TO ISSUES RAISED REGARDING SPECIFIC INFORMATION TO BE SUBMITTED

The following section outlines the applicant's response to the Board's request for 14 no. specific items of information which should be submitted with any application for permission. This additional information is requested as specified in Article 285(5)(b) of the Planning and Development (Strategic Housing Development) Regulations 2017 and Articles 297 and 298 of the Planning and Development (Strategic Housing Development) Regulations 2017.

2.1 Statement of Consistency

An Bord Pleanála requested the following information:

'A detailed statement of consistency and planning rationale, clearly outlining how in the prospective applicant's opinion, the proposal is consistent with the specific objectives of the Galway City Council Development Plan 2017 – 2023, in particular with respect to the Galway City Ring Road (GCRR) reservation corridor and to the specific objective for implementation of road widening and improvements at Bóthar na Choiste both to the west to the Castlegar Local Centre and east to the Tuam Road, in light of the concerns raised in the Planning Authority's opinion.'

2.1.1 Applicants Response

A Planning Report and Statement of Consistency has been prepared by MKO and submitted with this application in response to Item 1. In addition to this, a Statement of Consistency Matrix, responding to each relevant planning policy in relation to the proposed scheme has also been included in Appendix 4 of the Planning Report and Statement of Consistency.

Section 7.2.3 of the Planning Report and Statement of Consistency relates to the GCRR reservation corridor and Section 7.7.2.2 relates to the specific objective for road widening and improvements at Bóthar an Chóiste.

2.2 Material Contravention Statement

An Bord Pleanála requested the following information:

'A detailed statement, which should provide adequate identification of all such elements and justification as applicable, where the proposed development materially contravenes the Development Plan other than in relation to the zoning of the land, indicating why permission should, nonetheless, be granted, having regard to a consideration specified in section 37(2)(b) of the Act of 2000. In this regard, in addition to car parking contravention as identified by the applicant, consideration should also be given to possible contravention of the GCRR reservation route and plot ratio standards for new residential development as set out in the Galway City Development Plan 2017 – 2023.'

2.2.1 Applicants Response

A **Material Contravention Statement** has been prepared by MKO and submitted with this application in response to Item 2. This statement relates to three no. items within the scheme which are the subject of

a potential material contravention of the GCDP, which includes car parking provision, the GCRR reservation route and plot ratio standards.

2.3 Connection and Integration with Overall Area

An Bord Pleanála requested the following information:

‘An assessment on how the proposed scheme ties in with, connects and integrates with the expansion of the overall area. The subject site represents an expansion of the outer suburbs of Galway city centre at Castlegar. It is important that safe pedestrian and cycle connections to Galway city centre and to the development to the south is delivered. There needs to be strong permeability within the scheme and to the Castlegar local centre.’

2.3.1 Applicants Response

Details of how the proposed scheme ties in with, connects and integrates with the expansion of the overall area is detailed in:

- Section 3 of the Architecture Design Statement prepared by ONOM Architects
- Section 7.7.1 of the Planning Report and Statement of Consistency prepared by MKO
- DMURS Statement of Consistency prepared by Tobin Consulting Engineers
- Landscape Report prepared by CSR

The proposed development has been designed in accordance with the principles and guidance as set out in the Design Manual for Urban Roads and Streets (DMURS) 2019.

The main design objectives of the proposed development are as follows:

- Create a series of strong links to the adjacent amenities while providing a new local centre along the Bothar an Choiste road.
- Provide a new community facility/creche, which will be developed alongside an open space to interact with the Bothar an Choiste road and wider area.
- Create green habitat walkway links throughout the site connecting to the Proposed greenway link as per the Galway City development plan.
- Ensure the layout and design allow for pedestrian permeability for access to the larger recreational areas for all residents.
- Ensure site layout is optimised to provide passive surveillance to open areas which will discourage anti-social behaviour.

There is strong permeability both within the proposed development itself and through connections to the surrounding area. The street networks within the development have been designed to maximise connections between local areas and services. Pedestrians can gain access to all areas of the proposed development by way of the pathways and formal and informal crossings. This results in a continuous pedestrian route from all locations within the proposed development and in connections to the other local developments and amenities. The main pedestrian and cyclist access routes to the proposed development will be from the Bothar an Choiste Road. Pedestrians shall use the new pedestrian footpaths located along the Bothar an Choiste Road which will be extended to the proposed site entrance from the junction at local road L5041. These new footpaths will provide linkage to the existing footpaths along the local road L5041 which will allow pedestrians access bus stops, recreational areas and interact with surrounding residential developments.

2.4 Traffic and Transport Infrastructure

An Bord Pleanála requested the following information:

‘Further consideration of the traffic and transportation infrastructure upgrades required, who is responsible for delivery of upgrade and improvement of Bothar na Choiste, as per the specific objective set out in the CDP 2017 – 2023, and timeframe for delivery of the upgrades in tandem with delivery of residential development.’

2.4.1 Applicants Response

The proposals include the upgrade of 525m of the existing Bothar an Choiste road from the development to the junction at L5041. The upgrade works consist of road improvements, road widening and junction re-alignment, as agreed with Galway City Council. The road upgrade works will precede the housing development works and will be completed before any unit is moved into. Refer to the Architects **Dwg. 3078 Phasing Diagram**. for details. The road upgrade works are shown graphically on Tobin Consulting Engineers **Drawing no.s 10750-2113,2114 & 2115**.

Following pre-planning consultation with Galway City Council Roads and transport Department, the proposed upgrade of the existing Bothar an Choiste road will bring the road up to the required standard and in accordance with DMURS 2019.

The applicant, Lock House Developments Ltd, will be responsible for road improvements, road widening and junction re-alignment, as agreed with Galway City Council.

2.5 Car and Bicycle Parking Layout

An Bord Pleanála requested the following information:

‘Further justification of quantum and layout of car parking and bicycle parking.’

2.5.1 Applicants Response

A car and bicycle parking layout plan has been prepared by ONOM (**Dwg. 3076 Car & Bike Parking Provision Diagram**) and submitted with this application. As is illustrated in the Car & Bike Parking Provision Diagram, it is considered that the most appropriate quantum of car parking for a suburban site has been proposed.

All housing units are provided with grouped car parking spaces. Grouped car parking is provide to the main access routes with the footpath between the houses and parking spaces. This buffers the pedestrian from the street edge and also enhances the street animation by removing long private front gardens.

Bicycle parking is possible to the front and in the rear gardens of all dwelling units. Apartments have been provided with a rate of 1 Bike space per bedroom and 1 visitor bike space per 2 apartments as per ‘Sustainable Urban Housing: Design Standards for New Apartments’ (2018) section 4.17. It is proposed that several other visitor parking locations will be evenly distributed throughout the development.

Apartments are to be provided with 1 car parking space each and a visitor space per every 4 apartments, this is in line with the ‘Sustainable Urban Housing: Design Standards for New Apartments’ (2018) section 4.22. These spaces will be grouped and paved and closely landscaped to reduce the visual intrusion of the car-parking. There is a shortfall in the required carparking to the creche which is located adjacent to the duplex block adjacent to the site entrance. As many duplex spaces will be

vacated during office hours it is submitted that the shortfall can be made up with duplex visitor and regular spaces. This matter has been detailed further in the enclosed **Material Contravention Statement**.

2.6 Residential Amenity: Overlooking & Overshadowing

An Bord Pleanála requested the following information:

‘A report that addresses issues of residential amenity, specifically with regards to potential overlooking, overshadowing and overbearing. The report shall include full and complete drawings including levels and cross-sections showing the relationship between the proposed development and any adjacent existing or permitted development.’

2.6.1 Applicants Response

In accordance with Item 6, as requested above, a report which addresses the potential issue of residential amenity, specifically with regards to potential overlooking, overshadowing and overbearing, has been incorporated into the Planning Report and Statement of Consistency prepared by MKO, the Design Statement prepared by ONOM, and the Daylight, Sunlight and Overshadowing Study as prepared by IES, and furnished to the Board with this planning application.

The residential amenity and any potential issues in terms of overlooking, overshadowing and overbearing impact of the proposed development on both existing residents of adjoining developments and future occupants of the proposed development are assessed in the planning assessment, section 7, of the planning report and statement of consistency.

Overshadowing concerns have been considered and a Daylight, Sunlight and Overshadowing Study prepared by IES Limited Consultants, which has been furnished to the Board with this application. This study assesses the proposed development layout and building heights, and focusses on measuring the daylight impact on both the existing surrounding dwellings and internally on the site itself.

The Architectural Drawings pack includes several site section drawings which shows the relationship of the proposed scheme to the existing residential areas within close proximity to the proposed site. See **Drawings 3006-3012 Site Sections** for further details.

2.7 Daylight and Shadow Impact Assessment

An Bord Pleanála requested the following information:

A Daylight and Shadow Impact Assessment of the proposed development, specifically with regard to impact upon adequate daylight and sunlight for individual units, public open space, courtyards, communal areas, private amenity spaces and balconies.

2.7.1 Applicants Response

A Daylight, Sunlight and Overshadowing Study has been prepared by IES Limited Consultants and submitted with this application in response to item 7 of the Boards opinion.

2.8 Response to Planning Authority Opinion

An Bord Pleanála requested the following information:

'A full response to matters raised within the PA Opinion and addendum reports submitted to ABP on the 02.02.2022.'

2.8.1 Applicants Response

Galway City Council issued an opinion on the proposed scheme on the 2nd of February 2022. The full opinion is included in **Appendix 1** of this report.

Development Plan Policies and Objectives Opinion:

While, in principle the proposed residential development and creche is compatible with and contributes to the R residential land use zoning objective, there is a concern regarding the interface of the proposed development with specific development objectives of the CDP in particular the proposed N6 GCRR, a key strategic infrastructural development for the city. The boundary of the N6 GCRR reservation corridor shown on submitted site layout drawing(s) does not correspond with CDP maps, see figure no. 3. No reference is made in the submitted Planning Report and Statement of Consistency and the Traffic and Transportation Assessment to a section of the development being within the road reservation corridor of proposed N6 GCRR and its potential impact on the proposed development. And more importantly the development, as currently proposed, materially contravenes the Core Strategy of the CDP which specifically states that the Core Strategy is supported and informed by the Galway Transport Strategy and its objective to deliver the N6 GCRR for the future strategic development of the city and the development also contravenes the policies and specific development objectives of the COP under Chapter 3 Transportation. This element of the development should have been examined and justified in the submitted Material Contravention Statement. As with any proposed development along the N6 GCRR reservation corridor it is highly advisable that the N6 GCRR Project Office are fully consulted regarding the proposed development and any recommendations forthcoming included in any scheme.

The creche adheres to the childcare policies and the standards of the CDP as expressed under Section 7.4.3 Childcare especially in relation to outdoor play area requirements. However, it is noted that a shortfall in car parking provision occurs, 3 spaces.

Applications Response:

A **Material Contravention Statement** has been prepared by MKO and submitted with this application. This statement relates to the scheme's contravention of the N6 GCRR reservation corridor, car parking requirements and plot ratio.

Urban Design and Layout Opinion:

Concerns are expressed over a number of elements in the urban design and layout of the proposed development and the achievement of standards relating to Outer Suburbs Neighbourhoods, the urban design requirements expressed under Section 8.7 of CDP and Ministerial Planning Guidelines Urban Design Manual- A Best Practice Guide 2009. These points of concern are summarised as follows;

Entry into the estate:

The vista and immediate views into the estate, as one enters the main entrance of the estate is defined by a blank gable wall and rear garden boundary located 40 metres from entrance. As the main access for a residential development of 170 houses this is a poor vista, with no active frontage and does not create a distinctive sense of place as advocated in Urban Design Manual- A Best Practice Guide 2009, in particular criteria nos. 4 Variety, 6 Distinctiveness and 7 Layout.

Public open space 02:

The central area of public open space 02 and the private amenity open space of Block 03 will be subject to significant overshadowing by Block 03 in view of its orientation eroding the amenity value of this open space, which is also exacerbated by the placement of bin/bicycle stores on this space. It is respectively suggested that if this layout of public open space provision is deemed to be acceptable, that consideration be given to the repositioning of Block 03 to the opposite alignment along the open space, that is Block 03 to the western side of the open space. This will ensure improve infiltration of sunlight to the open space, limit overshadowing and provide linkage of this open space to the 'liner park' along the greenway public open space 03 and create an active street frontage and improve the amenity value of this open space. Bin and bicycle stores should not be placed on public open spaces as such structures reduce the amenity value of open spaces and block views creating poor visual outlooks.

Public Open Space 03:

A large section of the public open space 03, a frontage of approx. 40m in length, is not subject to natural surveillance. It is noted that criteria no. 8 Public Realm of the Urban Design Manual-A Best Practice Guide 2009, contends that all areas of open space should be designed to be inviting, safe and conveniently located for people's homes. Designers should therefore locate open space in areas where they will be directly overlooked by people when inside their home.

In addition, the function and the benefit to residents of the central area located between public open space 03 to public open space 04 is not clear, being an area dominated by car parking provision. This central area is not a pedestrian friendly space and desirable movements and crossing and for pedestrians/cyclists have not been fully addressed in the design of this space.

Overlooking of Cluain Riocard housing estate:

The site is at a higher ground level than the public road and Cluain Riocard housing estate to the south. In view of the topography of the land the proposed four storey apartment building at the south-western corner of the site will overlook the northern dwellings of Cluain Riocard as displayed by site section drawing no. 2003, site section 02-02. The increase in building height at this location in the context of prevailing topography means that privacy of the Cluain Riocard estate is not protected.

Car parking design:

The layout is dominated by car parking and hard landscaped areas that could accommodate unregulated car parking. Section 11.3.1(g) Car Parking Standards states that to prevent the area to the front of small scale apartment and townhouses developments being completely dedicated to car parking, the parking area shall be visually broken up. Car parking rows shall be broken up with trees, planters or some other feature which shall soften the visual impact of the car parking areas at a minimum interval of 6 car parking spaces. This design requirement has not been achieved by the current car parking space layout. Criteria no. 12 Detailed Design of the Urban Design Manual- A Best Practice Guide 2009 states that open car parking areas are considered as an integral element within the public realm design and are treated accordingly.

Private Open Provision:

Clarity regarding the provision of private amenity open space for proposed duplexes is required, in Block 3, 4 and S. This house type is not an apartment and therefore a private rear garden space which complies with the minimum CDP requirement of 50% of gross floor area of each duplex dwelling unit is required. The statement 'various' for information of private open space provision for each units under Appendix 3 Housing Quality Assessment by unit number of the submitted Architectural Design Statement provides insufficient detail and in addition details regarding floor areas, measurements and amenity areas therein should be clear, legible and easy to reference format.

Garden spaces in front of units and open to public view are not defined as private amenity open space under Section 11.3.1(c) Amenity Open Space Provision in Residential Developments of the CDP which

states private open space (areas generally not overlooked from a public road) exclusive of car spaces shall be provided at a rate of not less than 50% of the gross floor area of the residential unit. It is clear from site layout plan that these privacy strips are defined in the scheme as private amenity open space which would not comply with the standards of CDP.

Regarding Type B1/B2, B3 and C, front elevation drawing no. 2012 omits a front of house feature which is enclosed and dedicated to bike and bin storage, save for the 3d drawings in Section 04 in Architectural Statement. It is considered that scaled drawings of the proposed boundary treatment should be provided.

Balconies:

Balconies of proposed apartment blocks should not protrude forward of the wall of the building in view of the prevailing climatic conditions in Galway and the weather ability and safety of these features and should be enclosed/semi-enclosed into fabric of the building.

Pumping Station:

further detail is required regarding the design, visual appearance and landscaping of this area its associated impact on the street scene and scheme, limited detail is provided in drawings submitted particularly in relation to the change in ground levels in the scheme.

Pedestrian and cyclists access/links:

Further information regarding the levels, the design and landscaping of pedestrian/cyclists access links and associated open spaces is required in order to confirm are these linkages accessible, safe and convenient for all users. Clarity is required on the provision for safe and convenient pedestrian crossings that follow natural movement desire lines in particular between Block 02 and Apartment building.

Mobility concerns:

A particular concern is expressed over the distance of dwellings (approx. 24 dwellings) to car parking space provision. Such arrangements are not practical particular in terms of every-day life, especially for loading of shopping from the car, people with younger children and different mobility needs. Car parking spaces should be located in a convenient, accessible and safe location for residents in the interest of social inclusion.

Building Height:

Specific development Fig. 11.6 North of Bóthar na Choiste specifies that the layout of residential development and boundary treatment on these residential zoned lands shall have regard to the protected views from the Headford Road. This requirement should be considered in the proposed scheme noting the increase in building height in particular in relation to Block 04.

Finishes:

Finishes and materials should be hard wearing, weather proofed and the palette should reflect the local context and geology.

Applicants Response:

An **Architectural Design Statement** has been prepared by O'Neill O'Malley Architects and submitted with the application. The design statement outlines how the proposed development is consistent with each of the 12 Urban Design Criteria and addresses the concerns raised by the Planning Authority in relation to the urban design and site layout matters.

Overshadowing concerns have been considered and a **Daylight, Sunlight and Overshadowing Study** prepared by IES Limited Consultants, which has been furnished to the Board with this application.

Details of the proposed pumping station are provided in Section 6.2 of the submitted Civil Works Report and in **Dwg. no. 10750-2124** prepared by Tobin Consulting Engineers. The Landscape Masterplan (**Dwg. no. 20442-3-100**) prepared by Cunnane Stratton Reynolds illustrates that the pumping station area will be surrounded by a pollinator friendly native wildflower meadow and tree planting, where there will be a low frequency mowing regime.

Matters relating to mobility and parking management are addressed within the **Traffic and Transportation Assessment** prepared by Tobins Consulting Engineers.

Housing and Density Mix Opinion:

It is necessary to facilitate mixed residential development on zoned lands in this area of the city in order to support the delivery of an appropriate quantum of housing units in line with the future growth projections set out in the NPF, Galway MASP and the Core Strategy of the CDP. However, a plot ratio of the development 0.389:1 is below the 0.46:1 requirement in the CDP. The low plot ratio exhibited by the proposed scheme raises questions whether the scheme represents the sustainable use and optimal consolidation of existing serviced and zoned residential land, recognising the availability of a public transport service and existing local centre.

The mixture of house types and sizes of the development proposed is considered to be acceptable under the CDP. The proposed unit sizes reflect current demographic trends in household size in the city and nationally as referenced in Sustainable Urban Housing: Design Standards for New Apartments Guidelines for Planning Authorities (DHPLG December 2020) would satisfy the residential accommodation requirements of various future household categories.

Applicants Response:

The plot ratio within the scheme as submitted stands at 0.40:1. It is noted that this plot ratio of 0.40:1 is below the 0.46:1 maximum standard within the GCDP. A **Material Contravention Statement** has been prepared by MKO and submitted with this application. The potential contravention of the plot ratio requirement is addressed within this statement.

Parking Opinion:

Car parking provision is provided in grouped format and is readily accessible to residential units and creche. It is noted that there is a shortfall in car parking space provision of 8.5%, a stated 23 spaces. It is accepted that the proposed development is serviced by public transport however the site is located at edge of city/outer suburban location. Section 4.23 of the Apartment Design Guidelines, states that for all types of location, where it is sought to eliminate or reduce car parking provision, it is necessary to ensure, where possible, the provision of an appropriate number of drop off, service, visitor parking spaces and parking for the mobility impaired. Provision is also to be made for alternative mobility solutions including facilities for car sharing club vehicles and cycle parking and secure storage. It is also a requirement to demonstrate specific measures that enable car parking provision to be reduced or avoided. Section 3.4 Traffic Network: Car Parking of the CDP states that reduced car parking standard may also be considered acceptable for new development located along strategic public transport corridors depending on the prevailing level of service at that time. However concerns are expressed over the layout of the scheme in particular central area between character areas 01 and 02 and large areas of hard landscaping at south-western section of the site near apartment building. The design, public realm and landscape treatment of these spaces will render it be susceptible to uncontrolled car parking. It is advised if the car parking space provision proposed is found to be acceptable it should be subject to a comprehensive mobility and parking management plan and the design of public realm areas and roads being re-examined to limit uncontrolled car parking.

In principle the proposed bicycle parking facilities, spaces are located in the lower ground floor spaces of apartments provide secure, easy to use and readily accessible bicycle storage for residents in line with Section 4.17 of the Apartment Design Guidelines. However concerns are raised regarding outdoor bicycle and bin stores provided elsewhere in the scheme, two such structures erode the amenity value of public open area 02 and it is questionable if such facilities are located at optimum and convenient locations in the scheme. As stated previously, such storage facilities should not detract from communal public amenity open space areas. It is noted that the Report from the Transport Section raises concerns over cycle parking provision.

Applicants Response:

The shortfall of car parking within the scheme has been reduced from 23 spaces to 10 spaces (3.7%). There is a shortfall of 7 no. spaces at the duplexes and 3 no. spaces at the creche. It is noted that this shortfall in car parking materially contravenes the GCDP, and as such this issue has been addressed with a **Material Contravention Statement** as prepared by MKO.

The proposed development provides for a total 17 no. disabled parking spaces, successfully meeting the 5% requirement within the GCDP.

Matters relating to mobility and parking management are addressed within the **Traffic and Transportation Assessment** prepared by Tobins Consulting Engineers

Environmental Assessments Opinion:

Overall, the environmental reports submitted have assessed the development in the context of the source receptor pathway model, precautionary principle and physical and natural characteristics of the site. The best practice environmental control mitigation measures outlined in submitted reports are clearly described, and would be reasonable, practical and enforceable of the proposed development in particular the protection of bat pollutions, archaeological mitigation and water quality safeguards.

However, concerns are expressed regarding the Consideration of Significant Likely effect Air & Climate in the submitted Environmental Impact Assessment Screening Report, which does not examine the effects of noise. The north/northeastern section of the development lies within and adjoins the reservation corridor of the proposed N6 GCRR. 24 dwellings units; and associated bedroom accommodation and private amenity open spaces/rear gardens are likely to be subject to significant environmental noise associated with this proposed ring road. Policy 9.10 Air Quality and Noise of the CDP states that it is the policy of the Council to ensure the design of development incorporates measures to minimise noise levels in their design and reduce the emission and intrusion of any noise or vibration which might adversely impact on residential amenities, where appropriate. In addition, consider the Galway City Council Noise Action Plan 2019-2023 in the assessment of relevant development applications, where appropriate and implement environmental noise reduction measures as outlined in the Galway City Council Noise Action Plan. In view of residential amenities, this aspect of the development should be assessed in the Environmental Impact Assessment Screening Report, and also a Noise Impact and Acoustic Study accompanied with effective and appropriate noise attenuation measures submitted for the development. This issue of concern was raised previously in meetings.

Applicants Response:

An **Acoustic Design Statement** has been prepared by Amplitude Acoustics and submitted with this application. This assessment concludes that the overall development complies with the relevant requirements of the ProPG: Planning & Noise and the British Standard BS 8233:2014.

An updated **Environmental Impact Assessment Screening Report** (EIASR) has been prepared by MKO and submitted with this application. Noise impacts are addressed in section 3.7.1.4 of the EIASR.

Water, Environment, Parks and Transport Sections Opinions:

The specific requirements and recommendations of the Water Services, Environment, Parks and Transport Sections should be examined and addressed in the proposed development. Of particular note is the conclusion of the Transport Section Report which expresses concerns that the proposed development is premature in the context of the level of development of the L5041 Bothar an Choiste road to the east through to Castlegar village and the Tuam Road and there are no plans for the improvement of this public road.

Applicants Response:

Water Services Section

Within their opinion, the water services section have raised concerns relating to potable water supply, foul water supply, and surface water drainage. A **Civil Works Report** has been prepared by Tobin Consulting Engineers and submitted with the application. This report outlines that the proposed development has been designed in accordance with the principles of Sustainable Urban Drainage Systems (SuDS).

Environment Section

Details of the proposed domestic waste management strategy are provided within the **Building Lifecycle Report** prepared by O'Neill O'Malley Architects.

A **Construction & Demolition Waste Management Plan** has been prepared by Tobin Consulting Engineers in accordance with the Best Practice Guidelines for the Preparation of Resource & Waste Management Plans for Construction & Demolition Projects.

Parks Section

Details of the proposed tree and plant schedules and characters areas are provided on the **Landscape Masterplan** prepared by Cunnane Stratton Reynolds.

It is considered that there is no scope to provide additional tree planting at the creche without losing parking spaces or impacting the footpath. However, an additional planting/landscaping buffer has been provided across road from the creche.

The submitted Landscape Masterplan outlines that trees have been located to allow for future connections to neighbouring sites.

It is considered that a covered seating area adjacent to self-directed play equipment for 12–18-year-olds may raise the possibility of anti-social behaviour at this location. Therefore, it is proposed that the open space amenity grass will provide for informal play for all ages.

Transport Section

The proposed development will include the upgrade of the existing Bothar an Chóiste road from the proposed development to the junction at L5041 consisting of road improvements, road widening and junction re-alignment. Details of the proposed road upgrade are included in **Dwg. No. 10750-2113 'Proposed Road Upgrades'** prepared by Tobin Consulting Engineers.

This section of the road was recently the subject of a Compulsory Purchase Order by Galway City Council under Compulsory Purchase Order No. BnC-CPO-001, 2021. Galway City Council has, on the 23rd May 2022, made a Confirmation Order confirming without modification the above-named Compulsory Purchase Order.

A site-specific objective for the lands North of Bothar na Chóiste is provided within the Galway City Development Plan 2017-2023 under Fig. 11.6. This objective provides a requirement “*for road improvements capable of accommodating future developments shall be incorporated into any development proposals*”. The area relating to this site-specific objective is located primarily to the west of the subject site, as illustrated in **Figure 1** below. It is therefore considered that the proposed road upgrade has successfully addressed the site-specific objective within the GCDP, with the exception of a small section of the road to the east of the subject site as illustrated in **Figure 1** below.

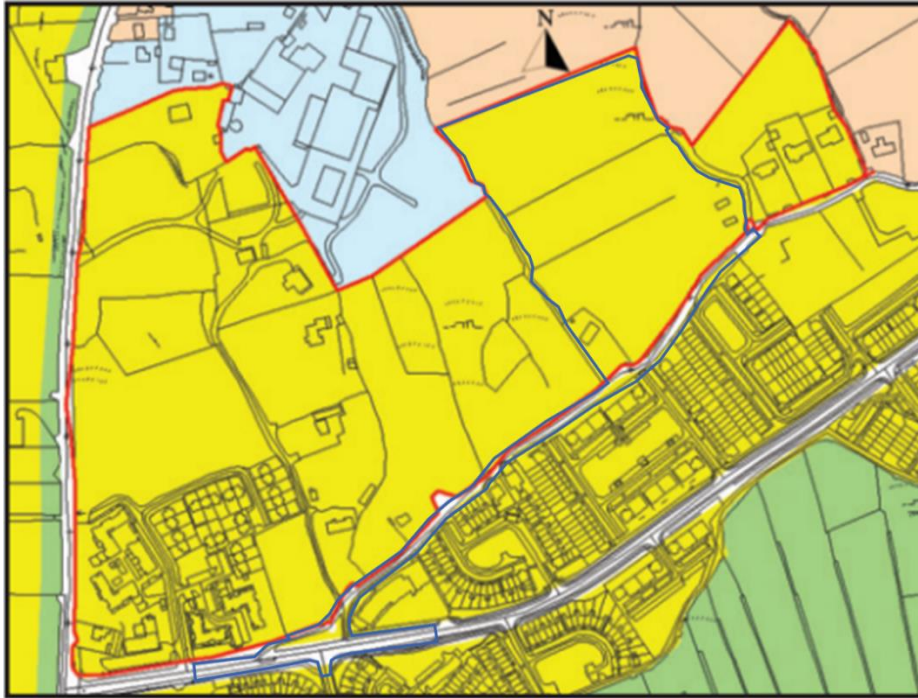


Figure 1: Fig 11.6 Lands to the North of Bothar na Chóiste with the subject site outlined in blue

Galway City Council have provided the applicant with a letter of consent for the proposed road upgrade works at Bothar an Choiste. This letter of consent is submitted with the planning application.

Details of swept path analysis are provided in **Dwg. 2110, 2111, and 2112** prepared by Tobin Consulting Engineers.

Response to Comment 1: A Traffic and Transportation Assessment and **Stage 1 Road Safety Audit** have been submitted with the application.

Response to Comment 2: A **Construction Traffic Management Plan** has been prepared by Tobin Consulting Engineers and submitted with the application.

Response to Comment 3: **Public Lighting Plans and a Site Lighting Report** have been prepared by Moloney Fox Consulting Engineers and submitted with the application.

Response to Comment 4: A Taking in Charge Plan has been prepared by ONOM and submitted as **Dwg. 3073 Indicative Taking in Charge Diagram**.

2.9

Landscaping Plans

An Bord Pleanála requested the following information:

‘Detailed landscape drawings that illustrate hard and soft landscaping, useable communal open space, meaningful public open space, quality audit and way finding. The public open

space shall be usable space, accessible and overlooked to provide a degree of natural supervision. Details of play equipment, street furniture including public lighting and boundary treatments should be submitted.'

2.9.1 Applicants Response

Dwg. 20442-3-100 prepared by Cunnane Stratton Reynolds provides a landscaping masterplan for the proposed development. This landscape masterplan provides details of hard and soft landscaping throughout the proposed development. A **Landscape Report** has also been prepared by Cunnane Stratton Reynolds and submitted with the application.

The landscape report and associated masterplan provides details of the layout of communal open spaces, play equipment, the communal garden, tree planting, and maintenance & management programme.

2.10 Noise Action Plan

An Bord Pleanála requested the following information:

'A Noise Action Plan and Assessment.'

2.10.1 Applicants Response

An **Acoustic Design Statement** has been prepared by Amplitude Acoustics and submitted with this application in response to item 10.

2.11 Building Lifecycle Report

An Bord Pleanála requested the following information:

'A full and detailed Building Lifecycle Report.'

2.11.1 Applicants Response

A **Building Lifecycle Report** has been prepared by O'Neill O'Malley Architects and submitted with this application. The Building Lifecycle Report provides an assessment of the long-term running and maintenance costs, and outlines the measures proposed to manage and reduce costs for residents.

2.12 Updated Ecological Impact Assessment

An Bord Pleanála requested the following information:

'An up-to-date Ecological Impact Assessment, inclusive of a Bird and Bat Survey.'

2.12.1 Applicants Response

An updated **Ecological Impact Assessment Report** (EcIA) has been prepared by MKO and submitted with this application. This report is inclusive of a bat survey and concludes as follows:

"Taking the above information into consideration and having regard to the precautionary principle, it is considered that the proposed development will not result in the loss of habitats

or species of high ecological significance and will not have any significant effects on the ecology of the wider area.

The potential residual impacts on ecological receptors will not be significant and no potential for the proposed development to contribute to any cumulative impacts on biodiversity when considered in-combination with other plans and projects was identified.

Provided that the development is constructed in accordance with the design and best practice that is described within this application, significant effects on biodiversity are not anticipated at any geographic scale.”

2.13 Taking in Charge Plan

An Bord Pleanála requested the following information:

‘A site layout plan indicating what areas, if any, are to be taken in charge by the planning authority.’

2.13.1 Applicants Response

A proposed layout for the taking-in-charge of the development has been included in this planning application (**Dwg. 3073 Indicative Taking in Charge Diagram**). This plan highlights those areas which are proposed to be taken in charge by the Local Authority, namely the vehicular routes through the site, the public footpaths, and the public open spaces.

2.14 Construction and Demolition Waste Management Plan

An Bord Pleanála requested the following information:

‘Site Specific Construction and Demolition Waste Management Plan.’

2.14.1 Applicants Response

A **Construction & Waste Demolition Management** Plan (CWDMP) has been prepared by Tobin Consulting Engineers. The CWDMP covers Waste Arisings: Demolition Waste, Excavation Waste and Construction Waste, Demolition Management, Waste Handling: On-site and Off-site, Record Keeping, Training, Responsibilities & Auditing and Interaction with Other Bodies.

3.

CONCLUSION

This document sets out how the items outlined in the Notice of Pre-Application Consultation Opinion from An Bord Pleanála in relation to the Strategic Housing Development proposed by Lock House Developments Ltd at Bóthar an Chóiste, Castlegar, have been addressed in full by the applicant and the design team prior to lodgement of the application.

It is therefore submitted that the proposed development, as updated to address the feedback provided by the local authority and An Bord Pleanála, is consistent with the proper planning and sustainable development of the area, and is consistent with all relevant national, regional and local planning policies and guidelines. The further information submitted should constitute a reasonable basis for an application for Strategic Housing Development.



APPENDIX 1

**GALWAY CITY COUNCIL
OPINION REPORT**



Comhairle Cathrach na Gaillimhe
Galway City Council

Halla na Cathrach
Bóthar an Choláiste
Gaillimh
H91 X4K8

City Hall
College Road
Galway
H91 X4K8

Our Ref: 20/04

Your Ref: ABP-312197-21

An Bord Pleanála,
64 Marlborough Street,
Dublin 1.

2nd February 2022.

For the attention of Cora Cunningham

**Re: L.A. Strategic Housing Development Ref. No. 20/04
An Bord Pleanála Ref. No. ABP-312197-21**

A Chara,

Further to your recent correspondence, please find enclosed copies of the following:

LA Opinion Report



Minutes of Meeting



Yours faithfully,

**Senior Executive Planner
Planning Department**

GALWAY CITY COUNCIL PLANNING SECTION

SHD 20/04 and ABP Ref 61.312197

Planning Opinion

Site Address: Bóther Na Chóiste, Castlegar

Applicant: Lock House Developments Ltd.

Description: The development consists of:

- 1) Construction of 139 no. residential units comprising; 38 no. duplex units (9 no. one bedroom units, 29 no. two bedroom units), 34 no. two storey two-bedroom houses, 42 no. two storey three-bedroom houses, 8 no. two storey four-bedroom houses, 1 no. four storey apartment block (10 no. one bedroom apartments and 7 no. two bedroom apartments).
- 2) Development of a creche facility (200 sqm), associated outdoor play areas and parking.
- 3) Road improvements to Bóther Na Chóiste.
- 4) Provision of shared communal and private open space, car and bicycle parking, site landscaping and public lighting, services, access with Bóther Na Chóiste, and all associated site development works.

The application will be supported by a Natura Impact Statement.

Zoning: R: To provide for residential development and for associated support development, which will ensure the protection of existing residential amenity and will contribute to sustainable residential neighbourhoods.

Site Area: 4.626 ha

1. Site Description

The subject site is located on the north-eastern Outer Suburbs of the city at Castlegar, to the north of Bóthar na Chóiste Road and east of N84 Headford Road. Largely a greenfield site, save for existing dwelling house at the south western corner of the site and a derelict dwelling and outbuilding located at the southern-eastern of the site. Site boundary are largely demarcated by stone walls and natural hedgerow. An agricultural laneway occurs along the south-eastern site boundary. In terms of topography, the land rises from south to north. The site is at higher elevation than road and housing estate Cluain Riocard to the south.

The site is located approximately 3.5 km north of Galway City Centre and approximately 300m from the local retail centre (Centra). Local bus route [407] has a bus stop outside the local centre. Castlegar National School is located approx. 0.8km to the north-east.



Figure 1 Location of site located to the north of Bóthar na Chóiste Road and east of N84 Headford Road.

There is no recent relevant planning history on site. Reference is made in documents to recent housing developments in the vicinity of the site; including pl. ref. no. 00/828 Cluain Riocard residential estate to the south, a development of 106 houses, 159 apartments, 4 entrances, a student accommodation facility consisting

of 31 student apartments, and pl. ref.no. 08/532 Cairéal Mór permission for the construction of 84 No. residential units (14 No. 1 bed apartments, 44 No. 2 bed townhouses and 26 No. 3 bed townhouses) in 8 No. two and three storey blocks, 118 No. car parking spaces located north/north west of the site.

2. Relevant City Development Plan Policy and Objectives

Land Use Policy

The site is zoned R: To provide for residential development and for associated support development, which will ensure the protection of existing residential amenity and will contribute to sustainable residential neighbourhoods in the current City Development Plan 2017-2023, (hereinafter referenced as CDP).

The policies for R residential zoned lands are outlined under Chapter 2 of the CDP entitled Housing and Sustainable Neighbourhoods. The site is located within the Outer Suburbs Neighbourhood of Ballybrit. Section 2.5 *Neighbourhoods: Outer Suburbs* states that successful residential development is reliant on the creation of neighbourhoods which have a distinct and special character, that are places which have recognisable features where people live and form an attachment. In the outer suburbs of the city, the creation of sustainable neighbourhoods will include for local shopping, community, leisure infrastructure and local employment opportunities. Access to public transport, walking and cycling is also an integral part of a sustainable neighbourhood.

New residential development will have regard to its surrounding context, provide for linkages with local facilities, a mix of house types and a layout that will provide for a high quality living environment. Infill development in the outer suburbs will have regard to the context of the area and will not adversely affect the character of an area. In addition the CDP advises that new housing development should respond to its context. The context includes the natural environment, the form of settlement, building and spaces, landscape features, contours, historical/archaeological features and local biodiversity.

Policy 2.5 *Neighbourhoods: Outer Suburbs* of the CDP seeks the following:

- Encourage higher residential densities at appropriate locations especially close to public transport routes and routes identified in the Galway Transport Strategy as suitable for high frequency, public transport services.
- Ensure that sustainable neighbourhoods are places where housing, streets, open spaces and local facilities come together in a coherent, integrated and attractive form.
- Ensure the layout of residential developments has regard to adjoining developments.
- Encourage a mix of housing types and sizes within residential developments.
- Encourage the use of homezones within residential developments.
- Require residential developments of over 10 units to provide recreational facilities as an integral part of the proposed open space.
- Ensure a balance between the reasonable protection of the residential amenities of the outer suburbs and the protection of the established character and the need to provide for sustainable residential development.
- Encourage the integration of energy efficiency in the design and layout of residential development.
- Encourage the promotion of universal design principles and lifetime adaptability in the design and layout of residential developments.
- Promote the use of appropriate placenames for new residential development in accordance with the policy of An Coiste Logainmneacha.

Regarding the proposed crèche/childcare facility, Chapter 2 Housing and Sustainable Neighbourhoods of the CDP acknowledges the contribution the provision of local services and community facilities bring residents together, create a focal point for the neighbourhood and reduce the need for traffic movements. The range of facilities can include schools, childcare facilities, community centres, primary health care centres and local commercial services.

Section 7.4.3 Childcare of the CDP states that in all new housing areas over 75 units, the provision of one childcare facility with a minimum of 20 childcare spaces is required, in line with the *Ministerial Guidelines for*

Planning Authorities on Childcare Facilities (DECLG 2001) and to create sustainable residential neighbourhoods. Exceptions to this will only be allowed, where there are substantiated reasons not to provide such a facility, for example if there are adequate childcare facilities in adjoining developments or the immediate area. The onus is on the developer to substantiate such exceptional cases.

Policy 7.4.3 Childcare of the CDP seeks to:

- Facilitate the development of childcare facilities, including after school services, at a number of suitable locations, such as, within residential areas, places of employment, city centre, neighbourhood and district centres, schools, in the vicinity of educational and community establishments and adjacent to public transport nodes.
- Contribute to the provision of childcare facilities by requiring that such facilities be provided in conjunction with residential developments over 75 dwelling units.
- Consider alternative arrangements where it can be clearly established that adequate childcare facilities exist.

Section 11.13 Childcare Facilities of the CDP outlines the development management standards for childcare facilities.

Specific Development Objectives

A number of lands zoned R residential in the city have the specific development objectives regarding design, environmental requirements and traffic safety. The subject site is part the land parcel subject to specific development objective Fig. 11.6 North of Bóthar na Cóiste which specifies the following requirements;

- Layout of residential development and boundary treatment shall have regard to the protected views from the Headford Road.
- Any development on these lands shall include for retention of the water body and incorporation of this feature into the development as an ecological amenity in addition to any open space requirements.
- Requirements for road improvements capable of accommodating future developments shall be incorporated into any development proposals.

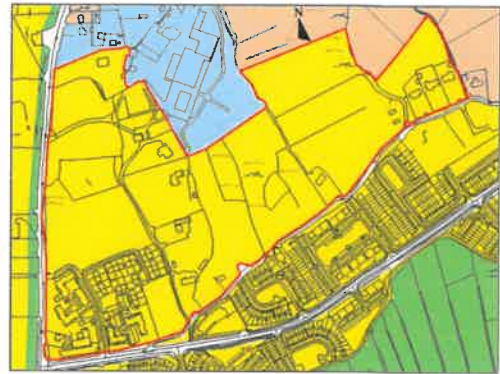


Figure 2 – Specific Objective Fig. 11.6 North of Bóthar na Cóiste

N6 GCRR

The north-eastern section of the scheme lies within the reservation corridor of the proposed N6 Galway City Ring Road (N6 GCRR). Policy 3.7 Road and Street Network and Accessibility of the CDP supports the N6 Galway City Ring Road project in conjunction with Galway County Council and Transport Infrastructure Ireland (TII) in order to develop a transportation solution to address the existing congestion on the national and regional road network. Section 3.10 Specific Objectives Traffic and Road Network states it is the objective of the CDP to reserve the preferred route corridor of the N6 Galway City Ring Road (N6 GCRR) project which has been selected to accommodate the requirements of an emerging strategic road and the associated bridge crossing of the River Corrib and give priority to the reservation of the N6 GCRR



Figure 3 Section of site lying within N6 GCRR reservation Corridor shaded in red.

Preferred Route Corridor and the associated land requirements over other land uses and objectives in the CDP and prohibit developments within the corridor which could potentially prejudice the development of this strategic road and river crossing.

Transportation

Section 3.10 of the CDP Specific Objectives Traffic and Road Network, states it is an objective of the Council to implement road widening and improvements at Bóthar na Cóiste.

In addition a specific development objective for a green way occurs along the western boundary of the site. Under Policy 4.5.1 Community Spaces; Greenways and Public Rights of way it is the policy of the Council to continue to develop and improve the greenway network in the city, providing alternative accessible circulation routes for pedestrians and cyclists for the enjoyment of the entire community.

Regarding the proposed bus route specific objective Section 3.5 *Public Transport Bus Network – Local* of the CDP states that the Galway Transport Strategy identified that a high quality, high frequency bus service is the most appropriate form of public transport provision for Galway City and the environs. Five core routes that would operate cross-city and seek to provide a minimum 15 minute frequency service during the peak periods and sustain a high frequency service throughout the day, this includes a service to Bóthar na Cóiste, see figure no. 5.

Local Centre

An existing local centre is located approx.300ms south-west of the site. Policy 5.1 Enterprise of the CDP facilitates the development of the designated District, Neighbourhood and Local centres at a scale appropriate to serve the associated catchment areas. These are defined under the Retail Hierarchy for Galway, see Chapter 6 Retail Strategy of the CDP as Level 6: Local Shopping – Villages; Residential Neighbourhoods in the City this level of provision is the most basic level and consists mainly of a mixture of corner and parade shopping in suburban areas. The CDP encourages these types of developments and retain existing ones in view of their contribution to sustainable neighbourhood development and because they afford access by sustainable transportation options.

Opinion:

While, in principle the proposed residential development and crèche is compatible with and contributes to the R residential land use zoning objective, there is a concern regarding the interface of the proposed development with specific development objectives of the CDP in particular the proposed N6 GCRR, a key strategic infrastructural development for the city. The boundary of the N6 GCRR reservation corridor shown on submitted site layout drawing(s) does not correspond with CDP maps, see figure no. 3. No reference is made in the submitted *Planning Report and Statement of Consistency* and the *Traffic and Transportation Assessment* to a section of the development being within the road reservation corridor of proposed N6 GCRR and its potential

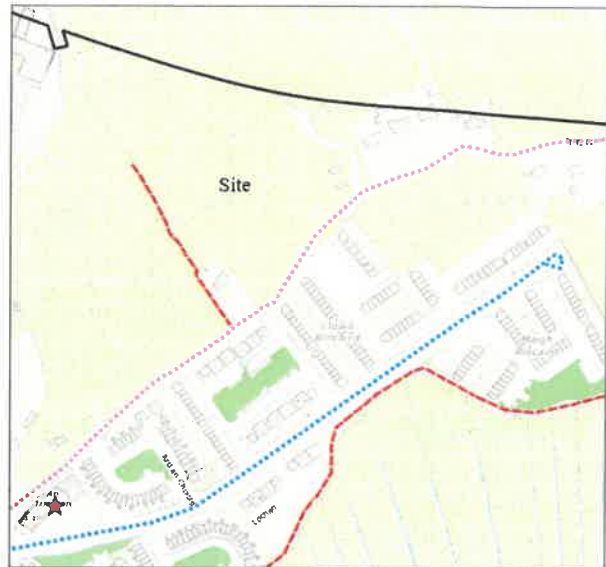


Figure 4 Specific Development Objective in the vicinity of the site. Red dashed line greenway, black line proposed N6 GCRR road reservation corridor, blue dotted line bus route, red star indicates existing local centre.



Figure 5 Fig 3.3 GTS taken from CDP, showing proposed Core Bus Routes Source: Galway Transport Strategy 2016. Note blue bus route to Bóthar na Cóiste

impact on the proposed development. And more importantly the development, as currently proposed, materially contravenes the Core Strategy of the CDP which specifically states that the Core Strategy is supported and informed by the Galway Transport Strategy and its objective to deliver the N6 GCRR for the future strategic development of the city and the development also contravenes the policies and specific development objectives of the CDP under Chapter 3 Transportation. This element of the development should have been examined and justified in the submitted *Material Contravention Statement*. As with any proposed development along the N6 GCRR reservation corridor it is highly advisable that the N6 GCRR Project Office are fully consulted regarding the proposed development and any recommendations forthcoming included in any scheme.

The crèche adheres to the childcare policies and the standards of the CDP as expressed under Section 7.4.3 Childcare especially in relation to outdoor play area requirements. However it is noted that a shortfall in car parking provision occurs, 3 spaces.

3. Urban Design & Layout

Section 8.7 *Urban Design* of the CDP contends that “*Good urban design within Galway City seeks to reinforce the distinctive character of the city by ensuring a high quality built environment through good place making and the creation of sustainable neighbourhoods*”. The CDP outlines the principles of good urban design, informed by Ministerial Guidelines including *The Sustainable Residential Development in Urban Areas (2008)*, *Urban Design Manual - A Best Practice Guide* includes principles and the *Design Manual for Urban Roads and Streets (DMURS) (2013)*. Section 11.3 of the CDP, *Standards for Residential Development*, seeks to protect residential amenities of established neighbourhoods and achieve high urban quality contributing to good place making.

The site is located within the Outer Suburbs Neighbourhood of Roscam. Section 2.5 *Neighbourhoods: Outer Suburbs* states that new residential development will have regard to its surrounding context, provide for linkages with local facilities, a mix of house types and a layout that will provide for a high quality living environment. Infill development in the outer suburbs will have regard to the context of the area and will not adversely affect the character of an area. In addition the CDP advises that new housing development should respond to its context. The context includes the natural environment, the form of settlement, building and spaces, landscape features, contours, historical/archaeological features and local biodiversity.

The DMURS sets out design approaches and standards moving away from the inflexible roads based design standards of the past to implement a more integrated approach to street design with the pedestrian at the top of user hierarchy.

The principles of good urban design are listed and elaborated in the CDP which include the following:

- Character
- Legibility
- Ease of movement and connectivity
- Quality of public realm
- Continuity and enclosure
- Diversity and adaptability
- Environmental responsibility

The *Urban Design Manual - A Best Practice Guide* set out the good urban design criteria for residential development including; context, connections, inclusivity, variety, efficiency, distinctiveness, layout public realm, adaptability, privacy/amenity, parking and detailed design.

Building Height

SPPR 4 of the *Urban Development and Building Height Guidelines for Planning Authorities (2018)* includes a specific planning policy requirement that in planning the future development of greenfield or edge of city/town locations for housing purposes, planning authorities must secure:

1. The minimum densities for such locations set out in the Guidelines issued by the Minister under Section 28 of the Planning and Development Act 2000 (as amended), titled “Sustainable Residential Development in Urban Areas (2007)” or any amending or replacement Guidelines;
2. A greater mix of building heights and typologies in planning for the future development of suburban locations; and
3. Avoid mono-type building typologies (e.g. two storey or own-door houses only), particularly, but not exclusively so in any one development of 100 units or more.

Communal Public Amenity Open Space

The *Sustainable Residential Development in Urban Areas (2008)*, sets out important qualitative standards for communal open space in terms of design, accessibility, variety, shared use, biodiversity, SuDs and community uses. Section 11.3.1 (c) *Amenity Open Space Provision in Residential Developments* of the CDP sets out the qualitative and quantitative requirements of residential developments shall provide for amenity open space areas including communal open space, which is required at a rate of 15% of the gross site area. Where acceptable ‘home zones’ are proposed, in accordance with Council Guidelines, the shared spaces shall be regarded as communal open space but shall not exceed one third of the total communal open space requirement. Shared spaces shall be regarded as communal open space where it is designed primarily to meet the needs of pedestrians, cyclists, children and residents and where the traffic speeds and dominance of the cars is reduced through design. The CDP states that communal open space in all types of residential development should:

- Be visually as well as functionally accessible to the maximum number of dwellings within the residential area.
- Be adequately overlooked by residential units.
- Integrate natural features (for example natural contours, outcrops of rock), where appropriate, as part of the open space.
- Be viable spaces, linked together where possible, designed as an integral part of the overall layout and adjoining neighbouring communal open spaces.
- Not include narrow pedestrian walkways, which are not overlooked by house frontages.
- Create safe, convenient and accessible amenity areas for all sections of the community.
- Generally no rear boundaries should face onto public open space. Blank gables shall not, generally face onto roads or streets. Side boundary walls, which face onto public open space, should be minimised.
- In all proposed residential development over ten units, a recreational facility shall be provided as part of the communal open space and funded by the developer. The recreational facility should be provided to serve the needs of the residents and should reflect the profile of future residents, the scale and type of development. Indicative examples of recreational facilities for different sizes of developments are shown in Table 11.2. The Parks and Amenity Department will give further guidance in relation to any proposed recreation facility.

Under Table 11.2 *Indicative Examples of Recreational Facilities for Different Sizes of Residential Developments* of the CDP, the requirements for development of 100+ residential units include for example playground, playing pitch, formal park.

Private Amenity Open Space

Regarding private open space provision the standards and requirements of the Apartment Design Guidelines and Section 11.3.1 (c) *Amenity Open Space Provision in Residential Developments* of the CDP shall apply to the apartments and standard house respectively. Section 11.3.1 (c) *Amenity Open Space Provision in Residential Developments* of the CDP states that private open space (areas generally not overlooked from a public road) exclusive of car spaces shall be provided at a rate of not less than 50% of the gross floor area of the residential unit.

Overlooking

In terms of overlooking, Section 11.3.1 (d) *Overlooking* of the CDP states that residential units shall not directly overlook private open space or land with development potential from above ground floor level by less than 11 metres minimum. In the case of developments exceeding two storeys in height a greater distance than 11 metres may be required, depending on the specific site characteristics.

Opinion:

Concerns are expressed over a number of elements in the urban design and layout of the proposed development and the achievement of standards relating to Outer Suburbs Neighbourhoods, the urban design requirements expressed under Section 8.7 of CDP and Ministerial Planning Guidelines *Urban Design Manual – A Best Practice Guide 2009*. These points of concern are summarised as follows;

Entry into the estate

The vista and immediate views into the estate, as one enters the main entrance of the estate is defined by a blank gable wall and rear garden boundary located 40 metres from entrance. As the main access for a residential development of 170 houses this is a poor vista, with no active frontage and does not create a distinctive sense of place as advocated in *Urban Design Manual – A Best Practice Guide 2009*, in particular criteria nos. 4 Variety, 6 Distinctiveness and 7 Layout.

Public open space 02

The central area of public open space 02 and the private amenity open space of Block 03 will be subject to significant overshadowing by Block 03 in view of its orientation eroding the amenity value of this open space, which is also exacerbated by the placement of bin/bicycle stores on this space. It is respectively suggested that if this layout of public open space provision is deemed to be acceptable, that consideration be given to the repositioning of Block 03 to the opposite alignment along the open space, that is Block 03 to the western side of the open space. This will ensure improve infiltration of sunlight to the open space, limit overshadowing and provide linkage of this open space to the 'liner park' along the greenway public open space 03 and create an active street frontage, and improve the amenity value of this open space. Bin and bicycle stores should not be placed on public open spaces as such strictures reduce the amenity value of open spaces and block views creating poor visual outlooks.

Public Open Space 03

A large section of the public open space 03, a frontage of approx. 40m in length, is not subject to natural surveillance. It is noted that criteria no. 8 Public Realm of the *Urban Design Manual – A Best Practice Guide 2009*, contends that all areas of open space should be designed to be inviting, safe and conveniently located for people's homes. Designers should therefore locate open space in areas where they will be directly overlooked by people when inside their home.

In addition the function and the benefit to residents of the central area located between public open space 03 to public open space 04 is not clear, being an area dominated by car parking provision. This central area is not a pedestrian friendly space and desirable movements and crossing and for pedestrians/cyclists have not been fully addressed in the design of this space.

Overlooking of Cluain Riocard housing estate

The site is at a higher ground level than the public road and Cluain Riocard housing estate to the south. In view of the topography of the land the proposed four storey apartment building at the south-western corner of the site will overlook the northern dwellings of Cluain Riocard as displayed by site section drawing no. 2003, site section 02-02. The increase in building height at this location in the context of prevailing topography means that privacy of the Cluain Riocard estate is not protected.

Car parking design

The layout is dominated by car parking and hard landscaped areas that could accommodate unregulated car parking. Section 11.3.1 (g) Car Parking Standards states that to prevent the area to the front of small scale apartment and townhouses developments being completely dedicated to car parking, the parking area shall be visually broken up. Car parking rows shall be broken up with trees, planters or some other feature which shall soften the visual impact of the car parking areas at a minimum interval of 6 car parking spaces. This design requirement has not been achieved by the current car parking space layout. Criteria no. 12 Detailed Design of the *Urban Design Manual – A Best Practice Guide 2009* states that open car parking areas are considered as an integral element within the public realm design and are treated accordingly.

Private Open Provision

Clarity regarding the provision of private amenity open space for proposed duplexes is required, in Block 3, 4 and 5. This house type is not an apartment and therefore a private rear garden space which complies with the

minimum CDP requirement of 50% of gross floor area of each duplex dwelling unit is required. The statement 'various' for information of private open space provision for each units under *Appendix 3 Housing Quality Assessment by unit number* of the submitted Architectural Design Statement provides insufficient detail and in addition details regarding floor areas, measurements and amenity areas therein should be clear, legible and easy to reference format.

Garden spaces in front of units and open to public view are not defined as private amenity open space under Section 11.3.1 (c) Amenity Open Space Provision in Residential Developments of the CDP which states private open space (areas generally not overlooked from a public road) exclusive of car spaces shall be provided at a rate of not less than 50% of the gross floor area of the residential unit. It is clear from site layout plan that these privacy strips are defined in the scheme as private amenity open space which would not comply with the standards of CDP.

Regarding Type B1/B2, B3 and C, front elevation drawing no. 2012 omits a front of house feature which is enclosed and dedicated to bike and bin storage, save for the 3d drawings in Section 04 in *Architectural Statement*. It is considered that scaled drawings of the proposed boundary treatment should be provided.

Balconies

Balconies of proposed apartment blocks should not protrude forward of the wall of the building in view of the prevailing climatic conditions in Galway and the weather ability and safety of these features and should be enclosed/semi-enclosed into fabric of the building.

Pumping Station

Pumping Station further detail is required regarding the design, visual appearance and landscaping of this area its associated impact on the street scene and scheme, limited detail is provided in drawings submitted particularly in relation to the change in ground levels in the scheme.

Pedestrian and cyclists access/links

Further information regarding the levels, the design and landscaping of pedestrian/cyclists access links and associated open spaces is required in order to confirm are these linkages accessible, safe and convenient for all users. Clarity is required on the provision for safe and convenient pedestrian crossings that follow natural movement desire lines in particular between Block 02 and Apartment building.

Mobility concerns

A particular concern is expressed over the distance of dwellings (approx. 24 dwellings¹) to car parking space provision. Such arrangements are not practical particular in terms of every-day life, especially for loading of shopping from the car, people with younger children and different mobility needs. Car parking spaces should be located in a convenient, accessible and safe location for residents in the interest of social inclusion.

Building Height

Specific development Fig. 11.6 North of Bóthar na Cóiste specifies that the layout of residential development and boundary treatment on these residential zoned lands shall have regard to the protected views from the Headford Road. This requirement should be considered in the proposed scheme noting the increase in building height in particular in relation to Block 04.

Finishes

Finishes and materials should be hard wearing, weather proofed and the palate should reflect the local context and geology.

¹ Units 98-100 in cell 04, units 124-137 in Block 03 and units 140-149 in Block 05.

4. Housing density and mix

At national and regional planning policy level, the future growth targets for Galway City are set out in the National Planning Framework (NPF) and reiterated in the Regional Spatial Economic Strategy (RSES) and the Galway Metropolitan Area Strategic Plan (MASP). Recent Variation No.5 to the CDP aligns its Core Strategy with the NPF, RSES and Galway MASP. The population targets set out in the MASP are for the Galway MASP area to grow by 27,500 to 2026 and by a further 14,500 to 2031, equating to a requirement for 10,110 and 5,330 new homes respectively, based on an average city household size of 2.72. A key sustainability element of the RSES and MASP is to deliver growth in a compact form with an objective to locate at least half of all new homes that are targeted, within the city footprint of the MASP. The redevelopment of key regeneration/brownfield sites in the city and development of infill sites and underutilised lands at locations that are well served by existing and planned public transport will support compact growth and population targets for the city.

Section 2.5 Neighbourhoods: Outer Suburbs states in relation to residential densities, the Sustainable Residential Development in Urban Areas (2008), promote higher residential densities in appropriate locations, especially in conjunction with public transport facilities subject to ensuring that the highest quality of residential amenity is achieved. In the design and assessment of higher density residential development, regard shall be had to these guidelines and the accompanying design manual which sets out certain criteria elaborated therein.

The Section 28 Ministerial Guidelines – Sustainable Urban Housing: Design Standards for New Apartments Guidelines for Planning Authorities, 2020 which set out requirements for size and design of apartments shall also be adhered to. The density of housing achieved in newer city neighbourhoods has been well balanced achieving the necessary critical mass to sustain essential infrastructure and sustainable communities. The subject site would be defined under the Sustainable Urban Housing: Design Standards for New Apartments Guidelines for Planning Authorities, 2020, as a Peripheral and/or Less Accessible Urban Locations, such locations are generally suitable for limited, very small-scale (will vary subject to location), higher density development that may wholly comprise apartments, or residential development of any scale that will include a minority of apartments at low-medium densities (will also vary, but broadly <45 dwellings per hectare net), including:

- Sites in suburban development areas that do not meet proximity or accessibility criteria;
- Sites in small towns or villages.

Section 2 of the Apartment Design Guidelines states that the identification of suitable sites to facilitate such higher densities would "...require local assessment that further considers these and other relevant planning factors". Apartments may be considered as part of a mix of housing types in a given housing development at any urban location, including suburbs, towns and villages

The plot ratio of the development (total gross floor area of residential development and crèche, 14,800.7m²) is calculated to be 0.389:1 based on a developable site area of 3.7988ha. Section 11.3.1 Outer Suburbs 11.3.1 (a) General states that a plot ratio of 0.46:1 for new residential development shall not normally be exceeded.

The strategy of the CDP as expressed under Chapter 2 Housing and Sustainable Neighbourhoods is to encourage sustainable neighbourhoods of high quality residential development, with a mix of house types and tenures with a strong identity and sense of place. Policy 2.2 Housing Strategy of the CDP states that planning applications for multiple housing units will be required to submit a Statement of Housing Mix detailing the proposed mix and demonstrating that it provides a sufficient minimum in house sizes and types to satisfy the demands of the emerging reduction in household sizes. The development standards for residential development in Outer Suburb neighbourhoods, Section 11.3.1 Outer Suburbs, 11.3.1 (a) General states that residential developments of 10 units and over shall normally provide a mix in type of residential units.

Regarding the proposed apartments, Sustainable Urban Housing: Design Standards for New Apartments Guidelines for Planning Authorities (DHPLG December 2020), (hereinafter referenced in this report as Apartment Design Guidelines) strives to enable a mix of apartment types that better reflects contemporary household formation and housing demand patterns and trends, particularly in urban areas. Specific Planning Policy Requirement 1 states that apartment developments may include up to 50% one-bedroom or studio type units

(with no more than 20-25% of the total proposed development as studios) and there shall be no minimum requirement for apartments with three or more bedrooms. Statutory development plans may specify a mix for apartment and other housing developments, but only further to an evidence-based Housing Need and Demand Assessment (HNDA), that has been agreed on an area, county, city or metropolitan area basis and incorporated into the relevant development plan(s).

Of the overall total 170 residential units; 5% are four bedroom units, 32% three bedroom units, 44% two bedroom units and 19% one bedroom units. Of total number of units, 50.5% are apartments. Regarding apartment unit mix, 37% of total apartment units are one bedroom, 48% two bedroom and 15% three bedroom.

Opinion:

It is necessary to facilitate mixed residential development on zoned lands in this area of the city in order to support the delivery of an appropriate quantum of housing units in line with the future growth projections set out in the NPF, Galway MASP and the Core Strategy of the CDP. However a plot ratio of the development 0.389:1 is below the 0.46: 1 requirement in the CDP. The low plot ratio exhibited by the proposed scheme raises questions whether the scheme represents the sustainable use and optimal consolidation of existing serviced and zoned residential land, recognising the availability of a public transport service and existing local centre.

The mixture of house types and sizes of the development proposed is considered to be acceptable under the CDP. The proposed unit sizes reflect current demographic trends in household size in the city and nationally as referenced in Sustainable Urban Housing: Design Standards for New Apartments Guidelines for Planning Authorities (DHPLG December 2020) would satisfy the residential accommodation requirements of various future household categories.

5. Parking

Car Parking

Section 11.3.1 (g) Car Parking Standards of the CDP provides in order to provide for flexibility in residential layouts the following options for car parking requirements:

- 2 on-site spaces per dwelling and 1 grouped visitor space per 3 dwellings or
- 1 on-site space per dwelling and 1 grouped visitor space per dwellings or
- 1.5 grouped spaces per dwelling and 1 grouped visitor space per 3 dwellings
- 3 spaces for dwellings over 200m² and 1 grouped visitor space per 3 dwellings
- 1 space for one bedroom residential dwellings and 1 grouped visitor per 3 dwellings

For crèche, see Table 11.5 Parking Space Requirement for Different Types of Development of the CDP which specifies 1 space per 20m² of operational space of the childcare facility.

The proposed layout provides for a total of 247 car parking spaces which includes the following; 242 spaces for residential use, and 5 spaces for crèche.

Bicycle Parking

Section 4.17 of the Apartment Design Guidelines states that accessibility to, and secure storage of, bicycles is a key concern for apartment residents and apartment proposals. Cycle storage facilities must be appropriate located to be directly accessible by residents and in secure storage facilities. In the current scheme, a total of 72 no. spaces are provided, consisting of; external bicycle shelters and internal designated bicycle storage spaces at the lower ground floor of proposed apartments.

Opinion:

Car parking provision is provided in grouped format and is readily accessible to residential units and crèche. It is noted that there is a shortfall in car parking space provision of 8.5%, a stated 23 spaces. It is accepted that the

proposed development is serviced by public transport however the site is located at edge of city/outer-suburban location. Section 4.23 of the Apartment Design Guidelines, states that for all types of location, where it is sought to eliminate or reduce car parking provision, it is necessary to ensure, where possible, the provision of an appropriate number of drop off, service, visitor parking spaces and parking for the mobility impaired. Provision is also to be made for alternative mobility solutions including facilities for car sharing club vehicles and cycle parking and secure storage. It is also a requirement to demonstrate specific measures that enable car parking provision to be reduced or avoided. Section 3.4 Traffic Network: Car Parking of the CDP states that reduced car parking standard may also be considered acceptable for new development located along strategic public transport corridors depending on the prevailing level of service at that time. However concerns are expressed over the layout of the scheme in particular central area between character areas 01 and 02 and large areas of hard landscaping at south-western section of the site near apartment building. The design, public realm and landscape treatment of these spaces will render it be susceptible to uncontrolled car parking. It is advised if the car parking space provision proposed is found to be acceptable it should be subject to a comprehensive mobility and parking management plan and the design of public realm areas and roads being re-examined to limit uncontrolled car parking.

In principle the proposed bicycle parking facilities, spaces are located in the lower ground floor spaces of apartments provide secure, easy to use and readily accessible bicycle storage for residents in line with Section 4.17 of the Apartment Design Guidelines. However concerns are raised regarding outdoor bicycle and bin stores provided elsewhere in the scheme, two such structures erode the amenity value of public open area 02 and it is questionable if such facilities are located at optimum and convenient locations in the scheme. As stated previously, such storage facilities should not detract from communal public amenity open space areas. It is noted that the Report from the Transport Section raises concerns over cycle parking provision.

6. Environmental Assessments

The proposed development is accompanied by a suite of environment assessments including; Natura Impact Statement, Appropriate Assessment, Ecological Impact Assessment, and Archaeological Impact Assessment. The site is located approx. 1.1m to the west as the crow flies from the nearest European Site, Galway Bay Special Area of Conservation Complex (site code 000268). In terms of ecology, there is no Annex I habitat recorded on site. Regarding built heritage there is no recorded archaeological national monument or protected structure on the site. The site is characterised as being in flood zone C with low probability of flooding. The site is however located in an area of extreme groundwater vulnerability (*source: Geological Survey Ireland Spatial Resources*) which is of particular relevance in the context of the adjoining Clada Group operation, north-west of the site, the provider of mineral water and soft drinks.

Opinion:

Overall the environmental reports submitted have assessed the development in the context of the source-receptor pathway model, precautionary principle and physical and natural characteristics of the site. The best practice environmental control mitigation measures outlined in submitted reports are clearly described, and would be reasonable, practical and enforceable of the proposed development in particular the protection of bat pollutions, archaeological mitigation and water quality safeguards.

However, concerns are expressed regarding the *Consideration of Significant Likely effect Air & Climate* in the submitted Environmental Impact Assessment Screening Report, which does not examine the effects of noise. The north/north eastern section of the development lies within and adjoins the reservation corridor of the proposed N6 GCRR. 24 dwellings units; and associated bedroom accommodation and private amenity open spaces/rear gardens are likely to be subject to significant environmental noise associated with this proposed ring road. Policy 9.10 Air Quality and Noise of the CDP states that it is the policy of the Council to ensure the design of development incorporates measures to minimise noise levels in their design and reduce the emission and intrusion of any noise or vibration which might adversely impact on residential amenities, where appropriate. In addition consider the Galway City Council Noise Action Plan 2019-2023 in the assessment of relevant development applications, where appropriate and implement environmental noise reduction measures as outlined in the Galway City Council Noise Action Plan. In view of residential amenities, this aspect of the

development should be assessed in the Environmental Impact Assessment Screening Report, and also a Noise Impact and Acoustic Study accompanied with effective and appropriate noise attenuation measures submitted for the development. This issue of concern was raised previously in meetings.

7. Opinions from other Sections of the City Council

The opinions and comments from other sections of the City Council namely Water Services, Environment, Parks and Transport Sections are detailed in Appendix 1 of this Report.

Opinion

The specific requirements and recommendations of the Water Services, Environment, Parks and Transport Sections should be examined and addressed in the proposed development. Of particular note is the conclusion of the Transport Section Report which expresses concerns that the proposed development is premature in the context of the level of development of the L5041 Bothar an Choiste road to the east through to Castlegar village and the Tuam Road and there are no plans for the improvement of this public road.

8. Concluding Opinion

On the basis of the foregoing, it is considered that while in principle, the proposed development complies with the R residential land use zoning objective, a number of concerns remain; including the location of the scheme in the boundary of N6 GCRR reservation corridor, urban design and layout issues, pumping station, environmental noise and shortfall and layout and quantum of car parking space provision. It is respectively suggested that the urban layout of the development be reviewed in the context of the *Urban Design Manual – A Best Practice Guide 2009*, and also the policies and development standards of the CDP in order to achieve an optimal layout for future residential community at this location. In addition, it is advised that the N6 GCRR Project Office are fully consulted regarding the proposed development and any recommendations forthcoming included in any scheme.



Diane Egan
Executive Planner
01/02/2022



SFP

11/2/2022

APPENDIX 1 OPINIONS FROM OTHER SECTIONS OF THE CITY COUNCIL

A1.1 Water Services Section

Report from the Mr. Frank Clancy A/Senior Executive Engineer, Water Services Section dated 13th of January 2022 states the following points:

Potable Water Supply

The applicant has submitted a Pre-Connection Enquiry Application to Irish Water and a letter of no objection has been forwarded to the applicant. An Irish Water connection agreement will be signed by the applicant at offer stage. The applicant is to lay a new 150mm watermain approximately 450metres and connect to the 200mm watermain along Bothar an Choiste.

There is an existing 75mm diameter water main which finishes circa 30m east from the proposed development entrance. The new 150mm main should be extended along the full length of the site frontage and connected to the existing via closed valve.

On Site Fire Storage is included in the design. Public Supply to be protected from backflow by unrestricted air gap to ISEN 1717.

Foul Water Supply

The applicant has submitted a Pre-Connection Enquiry Application to Irish Water and a letter of no objection has been forwarded to the applicant. An Irish Water connection agreement will be signed by the applicant at offer stage. The applicant proposed to lay a combination of a 100mm pumped rising main and a 225mm gravity sewer to connect into the public sewer along Bothar an Choiste. An onsite pump station is proposed which will be designed to cater for a future development of 100 units. The pump station is to be constructed in accordance with Irish Water Standard STD-WW-26.

Surface Water Drainage

The surface water drainage design for site water is designed to be contained within the site by means of soakpit design incorporating oil interceptors.

Potential conflict with 525 Trunk Watermain; The road from the site to Bothar na Choiste is to be upgraded and widened. The drawings so far do not include a longitudinal section. There is a 525mm diameter water main at this location which serves the entire East of Galway City. This main will have to be diverted so as to remove the conflict with the new road layout and proposed pipework to service this development. Given the importance of this main, the diversion will be carried out by an Irish Water appointed contractor.

A1.2 Environment Section

Report from Ms. Sharon Carroll, Executive Scientist, dated 6th of January 2022 states the following points;

In Galway City, there is a requirement for household waste to be segregated into 3 separate waste fractions - recyclable waste, residual waste and food/biodegradable waste, as per the Galway City Council (Segregation, Storage and Presentation of Household and Commercial Waste) Bye Laws 2019. These 3 waste fractions must be stored and collected separately. The documentation accompanying SHD 20/04 refers to bin storage facilities and 1100 litre refuse bins for blocks 2, 3 & 4 and for the apartment block. However, it does not include information relating to the number of bins that will be provided for each waste type. Each household must be provided with the facility to dispose of their food waste, their recyclable waste and their residual waste as separate waste fractions. It should be noted that food waste bins are not available in 1,100 litre capacity.

The Environmental Impact Assessment Screening Report refers to a Construction (Waste) Management Plan (CMP) which will be prepared and lodged with the formal application. Galway City Council promotes best

practice in the management of construction and demolition waste. All waste arising from site clearance and construction on site should be dealt with in compliance with the relevant waste and environmental legislation. Furthermore, I suggest that waste management options relating to the circular economy, the reuse of materials, waste prevention and waste reduction are considered in the proposed development. The EPA have recently published Best Practice Guidelines for the Preparation of Resource & Waste Management Plans for Construction & Demolition Projects.

A1.3 Parks Section

Report from Ms. Lisa Smyth, Landscape Architect, Recreation and Amenity Section, dated 21th of January 2022 states the following points;

Trees & Biodiversity

We are agreeable with the provision of trees and the quality of the planting proposal overall in the landscape drawings currently submitted. However, we note a number of risk factors which will likely undermine these proposals being achieved at construction stage:

There are design clashes between the drainage plan and landscape plan in a number of places throughout the site layout and as a result of this we calculate approx. 25-30 no trees will ultimately be omitted at planting stage. The majority of trees lost will be street trees with a number of large and medium sized trees being omitted generally (for example: there are 11 no. trees proposed around the pump station, however we estimate only 4 medium sized trees will realistically be planted here at construction stage). These design clashes will have a negative impact on: achieving an equal spread of tree canopy across the site; will impact the finish of the streetscapes; and will negatively impact the overall biodiversity gain of the development proposal. We require the applicant to confirm how the tree locations and the drainage layout will be achieved and to ensure tree planting numbers are secured as per the current proposal and will not be compromised.

Notwithstanding assurances of tree canopy cover across the site at design stage, tree planting proposals (and planting proposals in general) come under pressure again during the process of value engineering pre-construction. The process of value engineering must be viewed as significant risk factor to achieving Biodiversity gain and quality landscapes and streetscape and this must be prevented. An onus must be placed on the applicant/developer to ensure Biodiversity gain is achieved and the quality of the landscape proposal is not compromised by the process of value engineering, either by applying conditions to the grant of planning or by some other enforceable mechanism. Poor quality outcomes for landscapes undermine Biodiversity, communities and GCC's policies set out in the City Development Plan.

The tree schedule and plant pallet are contained in the landscape report but do not appear in drawing format. The applicant is required to include the plant schedules on drawings to ensure conditions attached to a grant of planning can be implemented at compliance stage and/or enforcement stage. Planting schedules and pallets will be to a level of detail as per Technical Guidance Note 2 (attached).

The street with the Children's Creche is underserved with street tree planting. The drainage layout in this area will also further exacerbate the number of trees possible on this street. The lack of tree cover combined with the 3 storey elevation of Block 2 Duplexes will result in a streetscape with a 'hard' finish. Ensuring tree canopy along this street is essential. We require the applicant to review this arrangement with a view to increasing tree numbers (for example; raised tree pits may be required in this location).

Permeability

Ensuring future permeability with neighbouring sites at clear locations was raised by GCC at previous meetings with the applicant. The proposed locations for these linkages are not indicated on the current drawings. The applicant is required to address GCC's concerns in this regard. Tree planting must not be located where future connections might occur. A revised layout clearly indicating permeability linkages must not reduce the number of trees as currently proposed.

Play provision & landscape character areas

The playground in the main open space will be designed for children aged 6-12 years with details of suitable equipment proposed on the landscape drawing (some details are contained in the landscape report, but do not appear in drawing format). Informal play provision for young children aged 0-6 years will be included in the communal open spaces directly adjacent to houses in the form of individual pieces of play equipment alongside a bench, and/or naturalistic play areas (with mounding, logs or rocks) beside seating and will be situated away from car parking or roads.

There is no play provision for the 12-18 years age group. The applicant is required to address this by the provision of covered seating beside self-directed play equipment located in a highly trafficked area (by pedestrians) and is well-lit and overlooked, and not adjacent to the playgrounds.

Items no. 6 & 7 will be indicated on the landscape plan and site layout plan.

Character areas

Different character areas are called up in relation to the built form on the site layout on the architects drawings. A similar approach should be applied to the landscape finishes, particularly in relation to proposed planting (especially street tree planting). The applicant is required to review the landscape plan with this in mind and to consider how tree species can underpin variety of streetscape and neighbourhood character while simultaneously enhancing the character of the built environment and creating a strong sense of place.

A Recreation & Amenity Section Technical Guidance Note 2 – Level of detail required in landscape drawings for Planning, Further Information and Compliance Stages accompanies the report.

A1.4 Transport Section

Report from Theo McLoughlin, Executive Engineer, Transport Section received on 1st of February 2022 stating the following points;

Site Access

The proposed site access is off Bothar an Choiste Rd Castlegar Galway. This existing road is a substandard road and will need to be brought up to the required standard.

Bothar an Choiste Rd. It is proposed to realign Bothar an Choiste Rd from the proposed entrance to the junction with Bothar an Choiste Rd to the south west, junction L5041 at Baile an Choiste

It is not intended to upgrade the Bothar an Choiste Road through to Castlegar village to the north east.

Internal Road

Internal roads throughout the site are generally 5.5 to 6.0m in width with footpaths located along either one or both sides of the road. The internal roads have been designed to avoid long straight section to discourage speeding.

Swept path analysis remains to be provided for newly aligned L5041 and internally within development

Cycle and pedestrian facilities

Within the site footpaths are located along either one or both sides of internal roads.

The area surrounding the development is rural in nature with limited pedestrian facilities and no dedicated cycle facilities.

The applicant has submitted DMURS statement showing fig 1 and fig 2 showing main walking and cycling linkages between the proposed development and the L5041 Baile an Choiste road. A proposed re-alignment and re-construction of the junction (Junction 2) will bring this junction to an acceptable arrangement for pedestrians.

Cycle Parking

It is proposed to provide 268 cycle parking spaces as part of the development.

Cycle parking is shown at various locations throughout the development close to apartment blocks. 72 no. spaces are provided associated with Apartments.

There does not appear to be provision for accessible cycle parking.

8 No. Cycle spaces are provided for at the crèche unit.

It is considered that cycle parking should be covered and secure and integral to the design of the development. Cycle storage for houses will be 168 for the 84 units with 20 no. spaces additional in public spaces.

There is no provision for Galway Coke Bike share scheme however this could be

Cars

The development is within 400m of existing bus route. This provides a context for reduced car parking provision. It is proposed to provide 247 car parking spaces as part of the development (cf 270 CDP requirement as elaborated in the proposal).

A Mobility Management plan has not been presented for the crèche facility.

No. "Car share" (GoCar) spaces are to be provided.

It is unclear how Ev charging facilities are to be managed. 2 no charging points are to be provided at the apartment blocks

It is noted that in the Development Plan the car parking shall not be exceeded however a reduction in car parking standards may be acceptable when an application for a development includes a travel plan. It is noted in the statement of Material contravention that the number of car parking spaces proposed represents a deficit of 23 No spaces in comparison with the CDP requirements. This is acceptable modest shortfall.

Traffic Impact

The developer has not presented analysis of an existing development for comparison purposes.

There is concern that the development will increase traffic flow on the sub-standard L5041 road Bothar an Choiste. No analysis is provided wrt traffic on Bothar an Choiste eastbound through to Tuam Road

Public Transport

There is an existing public transport route on the Baile an Choiste road within 400m of the development proposed.

Compliance with DMURS

A DMURS Statement of Compliance has been submitted with the application.

Public Lighting

The PL report submitted does provide a statement wrt the standard and classification of the lighting design. The report however does not have explicit specific regard to GCC adopted guidance document which is available to download from GCC website. Any departures from that standard should be explicitly elaborated and addressed. The report submitted makes careful and welcome reference to design accommodation for the existing local bat population including dimming and trimming.

The Lighting design as submitted is an acceptable basis to proceed for a wholly private development, however it should be noted that the proposed lanterns are not consistent with GCC stock requirements. In the event that it were to be proposed that the lighting to the development was to be TIC the units proposed would not be acceptable.

Taking in Charge

There are no areas to be taken in charge shown as part of the development.

Comment:

There remains concern that the proposed development is premature in the context of the level of development of the L5041 Bothar an Choiste road to the east through to Castlegar village and the Tuam Road. There are no plans for the improvement of this public road.

1. The TTA needs to be updated to include consideration of east bound L5041, Bothar an Choiste. Stage 1 / 2 Road safety audit is required for the development and Junction 1 and 2.
2. The developer shall submit a detailed construction traffic management plan to Galway City Council for agreement prior to the commencement of the works. The traffic management plan shall include proposals to prevent/minimize disruption to both vehicular and pedestrian traffic in the vicinity. No works shall commence until the traffic management plan is agreed with the Transportation Section, Galway City Council.
3. Details of public lighting scheme within the development and in the public realm shall be submitted to the Local Authority for agreement in writing before the development commences.
4. Prior to the commencement of development the applicant shall submit a taking in charge drawing for agreement with Galway City Council. All areas to be taken in charge must comply with the document Galway City Council Taking in Charge Policy for Private Housing Developments.

MINUTES OF MEETING

Bothar an Choiste, Castlegar

SHD 20/04

Date of Meeting: 09/12/2020

Attendance:

Caroline Phelan – Senior Planner

Liam Blake - Senior Executive Planner

Diane Egan - Executive Planner

Stephen Walsh – Senior Executive Parks Superintendent

Brendan Gallagher - Senior Executive Engineer, Transportation

Frank Clancy - Executive Engineer, Water Services

Theo McLoughlin - Executive Engineer Transportation

Joan Higgins, Staff Officer, Galway City Council

Pamela Harty – MKO

Sarah Hill – MKO

Brendan Heaney – Tobin

Richard Daly – Tobin

Michael Geraghty - Tobin

John O'Neill – O'Neill O'Malley

Padraic Clancy – Cooperative Housing

Keith Mitchell – Cunnane Stratton Reynolds

Tom Broderick - Applicant

Proposed Development:

139 units, creche facility.

Landscaping

Tobin's working on realigned road layout. Emphasis on open space_-Emphasis to potential future greenway. Semi naturalistic linear park with mix of regimes, giving balance between passive recreational parkland and ecological parkland. As much green space as possible.

GCC comment already piecing together various greenways. Right of Way to develop greenway – who will have title on the land?

Street front – access road being upgraded to 5.5m road, 1.8m path. Part of planning app.

SW requests to see design to ensure adequate margin for trees and to integrate with public lighting.

PC responds CHI responsible for maintenance and may have title but no issue with RoW.

Upgrading street front would be part of planning application.

Roads:

Road is 5.5m carriageway with 1.8m path reconfiguration of junction. Existing road reduced in width as traffic calming measure – doesn't give much scope for verges.

BG concerns re width of road and footpath – seems narrow. Like to see more detail.

CP – designated for road improvements not just access points.

SW – How do you see layout working to encourage off road movement?

LB – Part 8 refused some years ago because of access on roads issue.

Action – BG and BH to meet outside of meeting.

Water Services:

FC - No surface water sewer in area. No proper drainage or road. Road to east goes on 30/40 metres. Slope is down to that corner. Road is lower than site and will naturally drain to that point.

Water supply connection is fine. Might look at size of main. Bank between your site and Caireal Mor likely to be developed.

3 inch main finishes near your site – look to get connection made to that to close valve.

Trunk main crosses road and serves east of city along area where road widening is proposed. Looking to get AC Main under the road replaced. Don't want to leave a main under a new road.

Fire supply storage – supply from that to public service – ensure no backflow

BH – enquiry gone in to Irish Water.

GCC further comments:

Designed in manner that amenity area will be used as incidental car parking.

Design very bland – does not adhere to national guidelines.

Hard surfacing – not clear that spec is of a quality that will rescue hard surfacing.

Gables onto public realm not good interface.



Comhairle Cathrach na Gaillimhe Galway City Council

GALWAY CITY COUNCIL

SHD

01 FEB 2022

20/04

PLANNING SECTION

PLANNING REPORT SHD Stage 2 Report

RE: Planning Ref. No. SHD 20-4 TC61.312197 ABP-
Applicant: Lock House Developments Ltd
Location: Bothar an Choiste, Castlegar
Return Date: 24/1/2022

Development Description

Demolition of buildings, construction of 170 no. residential units (84 no. houses, 86 no. apartments), creche and associated site works.

Site Access

The proposed site access is off Bothar an Choiste Rd Castlegar Galway. This is a substandard road that will be upgraded as part of the development.

Bothar an Choiste Rd. It is proposed to realign Bothar an Choiste Rd from the proposed entrance to the junction with Bothar an Choiste Rd to the south west, junction L5041 at Baile an Choiste

It is not intended to upgrade the Bothar an Choiste Road through to Castlegar village to the north east.

Internal Road

Internal roads throughout the site are generally 5.5 to 6.0m in width with footpaths located along either one or both sides of the road. The internal roads have been designed to avoid long straight section to discourage speeding.

Swept path analysis remains to be provided for newly aligned L5041 and internally within development

Cycle and pedestrian facilities

Within the site footpaths are located along either one or both sides of internal roads.

The area surrounding the development is rural in nature with limited pedestrian facilities and no dedicated cycle facilities.

The applicant has submitted DMURS statement showing fig 1 and fig 2 showing main walking and cycling linkages between the proposed development and the L5041 Baile an Choiste road. A proposed re-alignment and re-construction of the junction (Junction 2) will bring this junction to an acceptable arrangement for pedestrians.

Cycle Parking

It is proposed to provide 268 cycle parking spaces as part of the development.

Cycle parking is shown at various locations throughout the development close to apartment blocks. 72 no. spaces are provided associated with Apartments.

There does not appear to be provision for accessible cycle parking.

8 No. Cycle spaces are provided for at the crèche unit.

It is considered that cycle parking should be covered and secure and integral to the design of the development. Cycle storage for houses will be 168 for the 84 units with 20 no. spaces additional in public spaces.

There is no provision for Galway Coke Bike share scheme however this could be

Cars

The development is within 400m of existing bus route. This provides a context for reduced car parking provision.

It is proposed to provide 247 car parking spaces as part of the development (cf 270 CDP requirement as elaborated in the proposal).

A Mobility Management plan has not been presented for the crèche facility.

No. "Car share" (GoCar) spaces are to be provided.

It is unclear how Ev charging facilities are to be managed. 2 no charging points are to be provided at the apartments blocks

There is no provision of Ev Charge point spaces ??

It is noted that in the Development Plan the car parking shall not be exceeded however a reduction in car parking standards may be acceptable when an application for a development includes a travel plan. It is noted in the statement of Material contravention that the number of car parking spaces proposed represents a deficit of 23 No spaces in comparison with the CDP requirements. This is acceptable modest shortfall.

Traffic Impact

The developer has not presented analysis of an existing development for comparison purposes.

There is concern that the development will increase traffic flow on the sub-standard L5041 road Bothar an Choiste. No analysis is provided wrt traffic on Bothar an Choiste eastbound through to Tuam Road

Public Transport

There is an existing public transport route on the Baile an Choiste road within 400m of the development proposed.

Compliance with DMURS

A DMURS Statement of Compliance has been submitted with the application.

Public Lighting

The PL report submitted does provide a statement wrt the standard and classification of the lighting design. The report however does not have explicit specific regard to GCC adopted guidance document which is available to download from GCC website. Any departures from that standard should be explicitly elaborated and addressed.

The report submitted makes careful and welcome reference to design accommodation for the existing local bat population including dimming and trimming.

The Lighting design as submitted is an acceptable basis to proceed for a wholly private development, however it should be noted that the proposed lanterns are not consistent with GCC stock requirements. In the event that it were to be proposed that the lighting to the development was to be TIC the units proposed would not be acceptable.

Taking in Charge

There are no areas to be taken in charge shown as part of the development.

Comment:

There remains concern that the proposed development is premature in the context of the level of development of the L5041 Bothar an Choiste road to the east through to Castlegar village and the Tuam Road. There are no plans for the improvement of this public road.

1. The TTA needs to be updated to include consideration of east bound L5041, Bothar an Choiste.
2. Stage 1 / 2 Road safety audit is required for the development and Junction 1 and 2.

3. The developer shall submit a detailed construction traffic management plan to Galway City Council for agreement prior to the commencement of the works. The traffic management plan shall include proposals to prevent/minimize disruption to both vehicular and pedestrian traffic in the vicinity. No works shall commence until the traffic management plan is agreed with the Transportation Section, Galway City Council.
4. Details of public lighting scheme within the development and in the public realm shall be submitted to the Local Authority for agreement in writing before the development commences.
5. Prior to the commencement of development the applicant shall submit a taking in charge drawing for agreement with Galway City Council. All areas to be taken in charge must comply with the document *Galway City Council Taking in Charge Policy for Private Housing Developments*.

B. Further Information:

Comment:

C. Refusal:

C. Refusal:

Comment:



Signed:

Theo McLoughlin BE C.Eng Chartered Engineer/Innealtóir Cairte, BE C.Eng
Innealtóir Feidhmiúcháin /Executive Engineer
An Rannóg Iompair / Transportation Department
Comhairle Cathrach na Gaillimh / Galway City Council



Comhairle Cathrach na Gaillimhe
Galway City Council

DATE: 21st Jan 2022

TO: THE PLANNING DEPARTMENT

RE: SHD PL. REF: 20/04, Stage 2

APPLICANT: Lock House Developments Ltd.



Application for Stage 2: Section 5 Pre-Application Consultation regarding a proposed Strategic Housing Development on Land at Bothar na Choiste, Castelgar, Co Galway

DEVELOPMENT DESCRIPTION:

Planning permission is sought for by Lock House Developments Ltd (the applicant) for development on a site which extends to 3,7988 ha on lands located to the north of Bothar na Choiste, in the townland of Castlegard, Galway. The Development will consist of the following: 1). Demolition of the existing house and derelict structures on site. 2). Construction of 170 no. residential units comprising: 84 no. two storey houses (34 no. two-beds, 42 no. three-beds, 8 no. four-beds); 1 no. apartment block comprising 17 no apartments (10 no. one-beds, 7 no. two-beds); 1 no. apartment block comprising 21 no. apartments (12 no. on-beds, 9 no. two-beds); 48 no. duplex units (11 no. one-beds, 24 no. two-beds, 13 no. three-beds). 3). Development of a two-storey creche facility with 40 no. child spaces (c. 291 sqm), associated outdoor play areas and parking. 4). Provision of all associated surface water and foul drainage services and connections including pumpingstation with all associated site works and ancillary services. 5). Provision of sharedcommunal and private open space, site landscaping and pubic lighting, resident and visitor parking including electric vehicle charging points, bicycle parking spaces along with pedestrian, cyclist and vehicular link throughout the developmetn, access with Bothar na Choiste and all associated site development works. 6). The application will be supported by a Natura Impact Statement (NIS).

A Chara,

With regard to the above SHD application Recreation & Amenity have made the following observations:

Trees & Biodiversity

We are agreeable with the provision of trees and the quality of the planting proposal overall in the landscape drawings currently submitted. However, we note a number of risk factors which will likely undermine these proposals being achieved at construction stage:

1. There are **design clashes between the drainage plan and landscape plan** in a number of places throughout the site layout and as a result of this we calculate approx. 25-30 no trees will ultimately be omitted at planting stage. The majority of trees lost will be street trees with a number of large and medium sized trees being omitted generally (for example: there are 11 no. trees proposed around the pump station, however we estimate only 4 medium sized trees will realistically be planted here at construction stage). These design clashes will have a negative impact on: achieving an equal spread of tree canopy across the site; will impact the finish of the streetscapes; and will negatively impact the overall biodiversity gain of the development proposal. We require the applicant to confirm how the tree locations and the drainage layout will be achieved and to ensure tree planting numbers are secured as per the current proposal and will not be compromised.
2. Notwithstanding assurances of tree canopy cover across the site at design stage, tree planting proposals (and planting proposals in general) come under pressure again during the **process of value engineering** pre-construction. The process of value engineering must be viewed as significant risk factor to achieving Biodiversity gain and quality landscapes and streetscape and this must be prevented. An onus must be placed on the applicant/developer to ensure Biodiversity gain is achieved and the quality of the landscape proposal is not compromised by the process of value engineering, either by applying conditions to the grant of planning or by some other enforceable mechanism. Poor quality outcomes for landscapes undermine Biodiversity, communities and GCC's polices set out in the City Development Plan.
3. The **tree schedule and plant pallet** are contained in the landscape report but do not appear in drawing format. The applicant is required to include the plant schedules on drawings to ensure conditions attached to a grant of planning can be implemented at compliance stage and/or enforcement stage. Planting schedules and pallets will be to a level of detail as per Technical Guidance Note 2 (attached).
4. The street with the Children's Creche is underserved with street tree planting. The drainage layout in this area will also further exacerbate the number of trees possible on this street. The lack of tree cover combined with the 3 storey elevation of Block 2 Duplexes will result in a streetscape with a 'hard' finish. Ensuring tree canopy along this street is essential. We require the applicant to review this arrangement with a view to increasing tree numbers (for example; raised tree pits may be required in this location).

Permeability

5. Ensuring future permeability with neighbouring sites at clear locations was raised by GCC at previous meetings with the applicant. The proposed locations for these linkages are not indicated on the current drawings. The applicant is required to address GCC's concerns in this regard. Tree planting must not be located where future connections might occur. A revised layout clearly indicating permeability linkages must not reduce the number of trees as currently proposed.

Play provision & landscape character areas

6. The playground in the main open space will be designed for children aged 6-12 years with details of suitable equipment proposed on the landscape drawing (some details are contained in the landscape report, but do not appear in drawing format). Informal play provision for

young children aged 0-6 years will be included in the communal open spaces directly adjacent to houses in the form of individual pieces of play equipment alongside a bench, and/or **naturalistic play areas** (with mounding, logs or rocks) beside seating and will be situated away from car parking or roads.

7. There is no play provision for the 12-18 years age group. The applicant is required to address this by the provision of covered seating beside self-directed play equipment located in a highly trafficked area (by pedestrians) and is well-lit and overlooked, and not adjacent to the playgrounds.
8. Items no. 6 & 7 will be indicated on the landscape plan and site layout plan.

Character areas

- Different character areas are called up in relation to the built form on the site layout on the architects drawings. A similar approach should be applied to the landscape finishes, particularly in relation to proposed planting (especially street tree planting). The applicant is required to review the landscape plan with this in mind and to consider how tree species can underpin variety of streetscape and neighbourhood character while simultaneously enhancing the character of the built environment and creating a strong sense of place.

Mise le meas,

LISA SMYTH

LANDSCAPE ARCHITECT (MILI),

RECREATION & AMENITY DEPARTMENT

Recreation & Amenity Section Technical Guidance Note 2 – *Level of detail required in landscape drawings for Planning, Further Information and Compliance Stages* (03.02.2021)

Planning Stage

Note to Applicant: If there are a number of trees on or adjacent to the development site you will be required to carry out a Tree Constraints Survey. This report will inform the design process and it should include general information regarding existing hedgerows. The Tree Constraints Survey will be carried out to BS5837 standard by a suitably qualified Arborist.

Landscape Drawings submitted as part of the planning application require the following level of detail from the landscape consultant:

1. Provide a landscape layout plan(s) detailing:

- Existing vegetation to be removed/retained indicated, including trees & hedgerows, as identified in Tree Constraints Survey and/or Ecologist Report (if one was required).
- Any protected areas on site or nearby (Sites of European, National and Local Ecological Importance), protected views, protected structures, and protected Trees indicated (with regard to Galway City Development Plan).
- Proposed and existing planting to be retained (with regard to the All-Ireland Pollinator Plan, Galway City Biodiversity Action Plan).
- Proposed passive & active recreation (with regard to Galway City Development Plan, relevant Local Area Plan, Recreation & Amenity Study, ROSPA/NPFA, and Design Standards for New Apartments 2018).
- Hierarchy of communal green spaces, hierarchy of pedestrian routes/crossings, hierarchy of planting (particularly trees), access and permeability of the site.
- Indicate location of attenuation tanks in relation to proposed large tree species.
- Demonstrate awareness of Irish Water networks in relation to proposed tree/ large shrub planting.
- Demonstrate awareness of proposed location of street lighting and locations of ESB sub-stations, mini pillars and overhead wires with regards to planting proposals and maintenance.

- Finished levels will be marked clearly on the drawing, including finished roof, road and ground levels to open spaces.
- If there is a significant change in level across the site, site sections will be required to illustrate this. These should include landscape finishes.
- Retaining walls will be clearly marked on the landscape plan.
- Maintenance pathways/access routes for hedgerow maintenance will be included.

2. Provide **soft landscape schedules** as follows:

- Planting schedules for Trees will include details of species, girth size, form (bare root/root ball), quantity and total number of plants.

PLANT SCHEDULE

Open Space Trees				
B/R = Bare Root (Winter planting), R/B = Rootball, C/G = Container grown				
Abv	Species	Size	Form	Quantity
AcE	Acer campestre Elsrijk (Field Maple Elsrijk)	10-12cm girth	B/R	3
AcE2	Acer campestre Elsrijk (Field Maple Elsrijk)	14-16cm girth	B/R	1
Al	Amelanchier lamarckii (multistem)	2-2.5m	R/B	4
Bp	Betula pendula (Silver Birch)	10-12cm girth	B/R	1
Bp2	Betula pendula (Silver Birch)	8-10cm girth	B/R	3

- Planting schedules for woodland/light woodland planting will include details of overall area, species, transplant form, density per sqm and total number of plants.
- Planting schedules for hedgerow planting will include type of planting (evergreen, native, mixed), single row or double staggered hedge, overall length, species, transplant form, density per linear meter, percentage mix of species and total number of plants.

OPEN SPACE BOUNDARY HEDGE PLANTING

Evergreen hedge 38 Linear metres					
Abv	Species	Size/form	Plant/ LinM	LinM	Quantity
H1/Exe	Eleagnus x ebbingei	5Ltr pot	2.5	38m	95
H2/Pla	Prunus lusitânica angustifolia	5Ltr pot	2.5	22m	55

- Planting schedules for general planted areas will include a preferred plant palette list demonstrating design intention with regard to species selection, shrub and perennial percentage mix, size/form and density. The design intention for general planted areas provided at planning stage should remain intact after a grant by Planning. Plant palettes are intended as a statement of design intent. A detailed planting plan will be required at Compliance Stage and will follow the original plant palettes provided at Planning Stage.

Plant Palette list below demonstrating design intent

OPEN SPACE - Bed 1 20sqm - Bark mulch to planted bed			
Shrubs, Grasses Ferns to Open Space Bed 1			
Abv	Species	Size	Density
ArtPC	Artemisia Powis Castle	2ltr	2
CalKF	Calama grastis Karl Foerster	3ltr	3
Chyt	Cholsya ternata	5ltr	1
EcSP	Erica carnea Springwood Pink	2Ltr	3
EcSW	Erica carnea Springwood White	2Ltr	3
LavH	Lavandula angustifolia Hidcote	2Ltr	3
PhEG	Phormium Evening Glow	5Ltr	1
Pstch	Polystichum setiferum	2ltr	2
Herbaceous perennials to Open Space Bed 1			
Abv	Species	Size	Density
AstS	Astilbe Sprite	2Ltr	5
DgtP	Digitalis purpurea	2Ltr	5
CroEmI	Crococsmia Emily McKenzie	2Ltr	3
Ergkrv	Erigeron karvinskianus	1Ltr	3
LibGf	Libertia grandiflora	1Ltr	3
CroI	Crococsmia lucifer	1Ltr	5

- Planting schedules will also include details of mulching to planted areas and if weed suppressing membrane will be used.

3. In line with the Authority's policy to implement the **All-Ireland Pollinator Plan** the plant schedules included will to ensure all new applications provide a high percentage (75%) of plants from the All Ireland Pollinator Plan and/or are suitable for native pollinators. This number should include a minimum of 35% herbaceous perennials in the mix, a generous portion of which should be suited to spring and late autumn flowering to support pollinators early and late in the season. Link below:

<https://www.biodiversityireland.ie/wordpress/wp-content/uploads/Pollinator-friendly-planting-code-temporary-draft.pdf>

4. Provide details of tree support (staking & guying) systems as required;
5. Provide a written **design statement** outlining the design intent and how the landscape proposal achieves the objectives set out in the Development Plan (as outlined in item 1. above). The design statement may be a separate document, or form part of the overall design statement for the design team.
6. Provide details of **hard landscaping materials** such as edging, kerbing and paving in palette format.
7. Images of planting, hard landscape finishes and amenity equipment may be shown on the drawing to support the design intent of the landscape plan.

Level of detail at Further Information and Compliance Stage

Note to Applicant: If the Authority feels the information contained in the initial submission to be insufficient the applicant may be required to provide **further information**. This information may be of a technical nature and may become part of any information required at **Compliance Stage**.

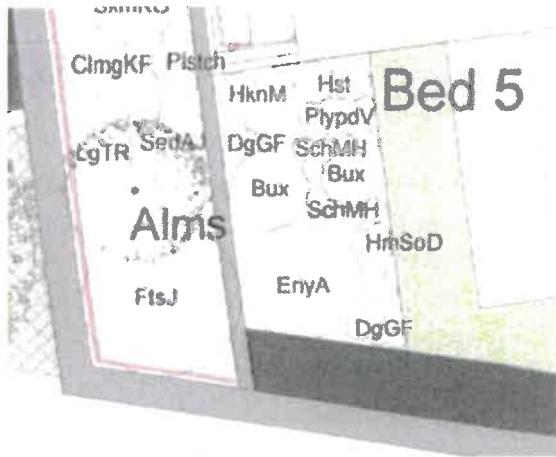
Information required at FI stage may include drawings with some or all of the following:

1. Any information not supplied as outline in **Sections 1). - 4). Planning Stage** (as above).

2. Some or all of the points in **Sections 3). – 7).** (below)

Information required at **Compliance Stage** may include drawings with some or all of the follow:

3. Provide a detailed **planting plans** (set out plan) indicating exact planting locations and expanded detail to areas of general planting, including planting schedules. Please note plant palette lists on Planning stage drawings will now be detailed planting schedules referencing the set out plan.



Example of planting plan/ set out plan

OPEN SPACE - Bed 1 20sqm - Bark mulch to planted bed					
Shrubs, Grasses Ferns to Open Space Bed 1					
Abv	Species	Size	Density	sqm	Quantity
ArtPC	Artemisia Powls Castle	2ltr	2	3	6
CalKF	Calamagrostis Karl Foerster	3ltr	3	2	6
Chyt	Choisya ternata	5ltr	1	4	4
EcSP	Erica carnea Springwood Pink	2Ltr	3	6	18
EcSW	Erica carnea Springwood White	2Ltr	3	4	12
LavH	Lavandula angustifolia Hidcote	2Ltr	3	3	9
PhEG	Phormium Evening Glow	5Ltr	1	1	1
Pstch	Polystichum setiferum	2ltr	2	3	6
Herbaceous perennials to Open Space Bed 1					
Abv	Species	Size	Density	sqm	Quantity
AstS	Astilbe Sprite	2Ltr	5	2	10
DgtP	Digitalis purpurea	2Ltr	5	2	10
CroEml	Crocoshia Emily McKenzie	2Ltr	3	3	9
Ergkrv	Erigeron karvinskianus	1Ltr	3	2	6
LibGf	Libertia grandiflora	1Ltr	3	3	9
CroL	Crocoshia lucifer	1Ltr	5	3	15

Example of plant schedule

4. Provide detailed **technical drawings** for all areas for planting including pit design, dimensions of pits/planted areas including composition of growth media to be used. Technical drawings will also include;

- Provide detailed design (drawings and specifications) of the drainage, irrigation & feeding systems that will be needed to sustain the vegetation proposed;
 - Provide details of any engineered soil or soil cell/strata cell type product (or similar approved) as required if tree pits are in or adjacent to paving/car parking
5. Provide details of location and extent of **Root Protection Area** as per the Tree Constraints Survey and details relating to fencing off of same during the construction phase.
 6. Further details relating to how the landscape plan has achieved the objective of the Development Plan with regard to amenity provision, biodiversity, climate change and the Recreation and Amenity Needs Study.
 7. Provide further details of **hard landscape materials** such as specifications and samples.
 8. Provide a **maintenance schedule** for the maintenance of the scheme throughout the season. The use of residual and systemic chemicals for maintenance purposes is not in-line with GCC's policy to ensure appropriate sustainable designs and management strategies. The use of herbicides, pesticides/algaecides must be avoided or subject to application restricted to only where essential where no other manual measures will prevail. The choice of planting will reflect sustainable design and management strategies, in accordance with the National Pollinator Plan (<http://pollinators.ie/resources/>).



By email

Comhairle Cathrach na Gaillimhe
Galway City Council

Water Services PLANNING REPORT

GALWAY CITY COUNCIL

13 JAN 2022

SHD 20/04

PLANNING SECTION

RE: Planning Ref. No. SHD 20/04
 Applicant: Lock House Developments Ltd
 Return Date: 13 January 2022

Potable Water Supply

The applicant has submitted a Pre-Connection Enquiry Application to Irish Water and a letter of no objection has been forwarded to the applicant. An Irish Water connection agreement will be signed by the applicant at offer stage. The applicant is to lay a new 150mm watermain approximately 450metres and connect to the 200mm watermain along Bothar an Choiste. There is an existing 75mm diameter water main which finishes circa 30m east from the proposed development entrance. The new 150mm main should be extended along the full length of the site frontage and connected to the existing via closed valve.

On Site Fire Storage is included in the design. Public Supply to be protected from backflow by unrestricted air gap to ISEN 1717.

Foul Water Supply

The applicant has submitted a Pre-Connection Enquiry Application to Irish Water and a letter of no objection has been forwarded to the applicant. An Irish Water connection agreement will be signed by the applicant at offer stage. The applicant proposed to lay a combination of a 100mm pumped rising main and a 225mm gravity sewer to connect into the public sewer along Bothar an Choiste. An onsite pump station is proposed which will be designed to cater for a future development of 100 units. The pump station is to be constructed in accordance with Irish Water Standard STD-WW-26.

Surface Water Drainage

The surface water drainage design for site water is designed to be contained within the site by means of soakpit design incorporating oil interceptors.

Potential conflict with 525 Trunk Watermain;

The road from the site to Bothar na Choiste is to be upgraded and widened. The drawings so far do not include a longitudinal section. There is a 525mm diameter water main at this location which serves the entire East of Galway City. This main will have to be diverted so as to remove the conflict with the new road layout and proposed pipework to service this development. Given the importance of this main, the diversion will be carried out by an Irish Water appointed contractor.

Signed:

Frank Clancy
A Senior Executive Engineer, Water Services

INTERNAL MEMO

TO: Anne Hackett, A/Staff Officer, Planning Department
FROM: Sharon Carroll, Executive Scientist, Environment Department
DATE: 6th January 2022

Re: Applicant: Lock House Developments Ltd.
Galway City Council Ref: SHD 20/04
Strategic Housing Development in respect of lands at Bothar na Choiste, Castlegar

Dear Anne,

In response to your letter dated 4th January 2022 requesting input into SHD 20/04, I have reviewed the relevant documentation and my comments are as follows:

- In Galway City, there is a requirement for household waste to be segregated into 3 separate waste fractions - recyclable waste, residual waste and food/biodegradable waste, as per the Galway City Council (Segregation, Storage and Presentation of Household and Commercial Waste) Bye Laws 2019. These 3 waste fractions must be stored and collected separately. The documentation accompanying SHD 20/04 refers to bin storage facilities and 1100 litre refuse bins for blocks 2, 3 & 4 and for the apartment block. However, it does not include information relating to the number of bins that will be provided for each waste type. Each household must be provided with the facility to dispose of their food waste, their recyclable waste and their residual waste as separate waste fractions. It should be noted that food waste bins are not available in 1,100 litre capacity.
- The Environmental Impact Assessment Screening Report refers to a Construction (Waste) Management Plan (CMP) which will be prepared and lodged with the formal application. Galway City Council promotes best practice in the management of construction and demolition waste. All waste arising from site clearance and construction on site should be dealt with in compliance with the relevant waste and environmental legislation. Furthermore, I suggest that waste management options relating to the circular economy, the reuse of materials, waste prevention and waste reduction are considered in the proposed development. The EPA have recently published Best Practice Guidelines for the Preparation of Resource & Waste Management Plans for Construction & Demolition Projects.

Mise le meas

Sharon Carroll



