



Lock House Developments Ltd.

Strategic Housing Development at Bóthar an Chóiste,
Castlegar, Galway

PRELIMINARY CONSTRUCTION TRAFFIC
MANAGEMENT PLAN



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1.0 INTRODUCTION

This preliminary Construction Traffic Management Plan (CTMP) has been prepared as part of planning application for the development proposed strategic residential development at Bothar an Chóiste, in Galway City. This document has been produced to outline a plan for the management of construction traffic during the works on this project.

This document has been compiled for Planning Stage purposes and the final Site-Specific Construction Traffic Management Plan will be produced by the appointed Contractor and PSCS in conjunction with the PSDP for the project. This Report outlines the preliminary management plan and what will be expected of the Main Contractor's Management Plan at construction stage.

This document has been prepared with reference to the following guidance documents:

- Galway City Development Plan 2017-2023
- Department of Transport "Traffic Management Guidelines"
- Department of Transport "Traffic Signs Manual"
- Commission of the European Communities Green Paper "Towards a New Culture for Urban Mobility"
- Dublin Transport Office Advice Note on Mobility Management Plans.
- Best Practice Guidelines on the Preparation of Waste Management Plans for Construction and Demolition Projects by the Department of Environment Heritage and Local Government.

1.1 PURPOSE

This preliminary CTMP will be provided to the Main Contractor for implementation during the site clearance and construction stages. It will be considered as a 'Live Document' and will be updated accordingly throughout the project as required.

On appointment, the Main Contractor is required to implement the mitigation and protective measures set out in this document and maintain traffic monitoring records for the duration of the project which shall be made available to representatives from Galway City Council for inspection on request.

2.0 DESCRIPTION OF THE PROJECT

The development will consist of the following:

- 1) Demolition of an existing house (124.6 m²), a ruined outbuilding (42.8 m²), and a ruined dwelling (41.7 m²)
- 2) Construction of 170 no. residential units comprising:
 - 84 no. two storey houses (34 no. two-beds, 42 no. three-beds, 8 no. four-beds),
 - 1 no. apartment block comprising 17 no. apartments (10 no. one-beds, 7 no. two beds),
 - 1 no. apartment block comprising 21 no. apartments (12 no. one-beds, 9 no. two beds),
 - 48 no. duplex units (11 no. one-beds, 24 no. two-beds, 13 no. three-beds).



- 3) Development of a two-storey creche facility with 46 no. child spaces (c. 300.36 sqm), associated outdoor play areas and parking.
- 4) Provision of all associated surface water and foul drainage services and connections including pumping station with all associated site works and ancillary services.
- 5) The upgrade of the existing Bothar an Chóiste road from the proposed development to the junction at L5041 consisting of road improvements, road widening and junction re-alignment.
- 6) Pedestrian, cyclist, and vehicular links throughout the development and access with Bóthar Na Chóiste, and pedestrian and cyclist link to the adjacent Greenway route.
- 7) Provision of shared communal and private open space, site landscaping and public lighting, resident and visitor parking including electric vehicle charging points, bicycle parking spaces, and all associated site development works.
- 8) The application is accompanied by a Natura Impact Statement (NIS).

The site location and proposed site development layout are shown in the following Figures.



Figure 2-1: Proposed Site Location©google



Figure 2-2: Proposed Site Development Layout

2.1 EXISTING ROAD NETWORK

The proposed development can be accessed from Bothar an Chóiste, which links to the L5041 local road and the N84 national secondary route. The proposed access into the development will be from a new priority T-junction on the Bothar an Chóiste Road. The proposed site access will be situated within a design speed zone of 50km/h. The existing Bothar an Chóiste road adjacent to the proposed development has a carriageway width which ranges from 4.0 – 6.0m. Separately, improvement works to the existing Bothar an Chóiste road and junction are also proposed.

3.0 TRANSPORTATION FACILITIES

3.1 ROAD NETWORK AND ACCESS

Traffic will access the site along the Bothar an Chóiste road. Access to the site will be from the southwest direction as the road to the east is not sufficient for carrying HGV traffic.

3.2 PROPOSED SITE ACCESS JUNCTION & BOTHAR AN CHÓISTE UPGRADE WORKS

Access to the proposed development site will be gained through a new priority junction onto the existing Bothar an Chóiste Road. Upgrade works are also proposed for the Bothar an Chóiste Road and the L5041 / Bothar an Chóiste Road junction as the road and junction layout is unsafe for vulnerable users and is not in accordance with the relevant road design standards. The proposed junction layout will regularise traffic movements to and from Bothar an Chóiste and the road upgrade will allow for safer pedestrian access to the proposed development site. Significant engagement has been undertaken with Galway City Councils Roads Department throughout the design process to ensure adherence with their requirements.

The design principles of the Design Manual for Urban Roads and Streets (2019) apply to the proposed development and the L5041 / Bothar an Chóiste junction as they are both located within the 50km/h urban speed zone for Galway City. The design of all new accesses / upgrade works will take account of this design guidance. The proposed layout for the L5041 / Bothar an Chóiste Junction is outlined in the Figure below.

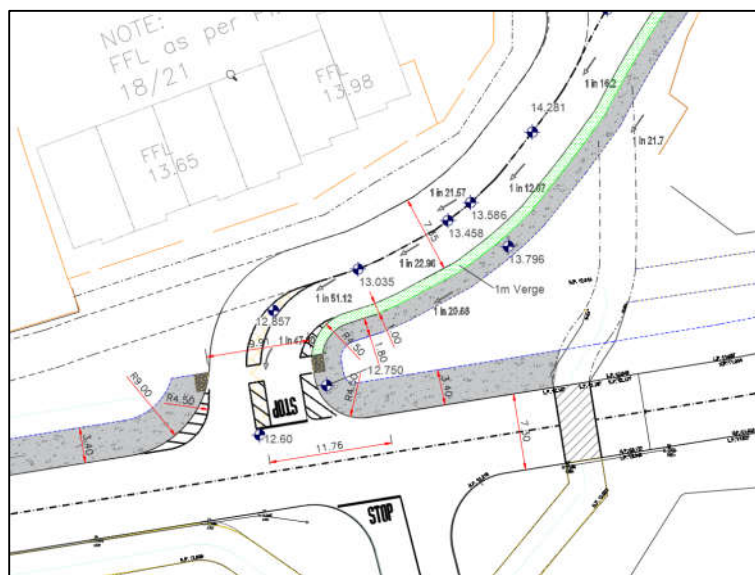


Figure 3-1: Proposed L5041 / Bothar an Chóiste Junction Layout

The extent of the existing Bothar an Chóiste road to be upgraded is shown in the following Figure:



Figure 3-2: L-5041 Bothar an Chóiste Extent of Upgrade Works

3.3 CAR PARKING

Provision will be made onsite for a construction staff carpark within the Contractor's compound.

3.4 PEDESTRIANS AND CYCLISTS

3.4.1 PEDESTRIAN FOOTWAYS

There is a lack of facilities for vulnerable road users (pedestrians and cyclists) along the existing route.

The Bothar an Chóiste walking and cycling network is comprised of existing footpaths adjoining public roads. The main approaches along the L5401 have shared footpaths for pedestrians and cyclists. The existing footpath continues 100m approx. along the Bothar an Chóiste road. There is currently no dedicated footpath or cycling network between the proposed Bothar an Chóiste site entrance and the L5041.

As part of the proposed Bothar an Chóiste road upgrade works, the existing footpath on the L5410 is to be extended up to the entrance of the proposed development. Drawing 10750-2113 demonstrate the proposed linkage with a 1.8m wide footpath located on the southern side of the Bothar an Chóiste road.

3.5 PUBLIC TRANSPORT

The Bothar an Chóiste / Castlegar area is served by the Bus Eireann 407 local bus route, which operates a frequent service to and from Galway City Centre. Bus route 407 stops at the Árd An Chóiste bus stop, approximately a 6-minute walk from the proposed site. This bus route travels from Árd an Chóiste and Galway City and runs Monday to Sunday.

Monday to Friday it operates from Árd an Chóiste to Eyre Square between 07:15 and 23:45, on Saturdays it operates between 07:45 to 23:45 and on Sundays it operates between 08:46 and 24:46.

Monday to Friday it operates from Eyre Square to Árd an Chóiste between 06:45 and 23:15, on Saturdays it operates between 07:15 to 23:15 and on Sundays it operates between 08:15 to 23:15. Provision will be made by the Main Contractor to ensure the bus services are maintained at all times

4.0 TRAFFIC MANAGEMENT

4.1 TRAFFIC MANAGEMENT PLAN

This section outlines the content of the final Construction Traffic Management Plan (CTMP) which shall be prepared prior to construction of the proposed development. It shall be a requirement of the contract that, prior to construction, the appointed Contractor shall liaise with the relevant authorities including Galway City Council, TII and Emergency Services for the purpose of finalising the CTMP, which will encompass all aspects of this preliminary Traffic Management Plan. The CTMP shall be termed a 'Live Document', such that any changes to construction programme or operations can be incorporated into the CTMP.

The Contractor will be contractually required to ensure that the elements of this preliminary CTMP shall be incorporated into the final CTMP. The principal contractor shall also agree and implement monitoring measures to confirm the effectiveness of the mitigation measures outlined in the CTMP. On finalisation of the CTMP, the contractor shall adopt the plan and associated monitoring measures. The final CTMP shall address the following issues (including all aspects identified in this preliminary CTMP):

- Site Access & Egress.
- Traffic Management Signage.
- Routing of Construction Traffic / Road Closures.
- Timings of Material Deliveries to Site.
- Traffic Management Speed Limits.
- Road Cleaning.
- Road Condition.
- Road Closures.
- Enforcement of Traffic Management Plan
- Details of Working Hours and Days.
- Details of Emergency plan.
- Communication.
- Construction Methodologies; and
- Construction Impacts

These items are explained in detail in the remainder of this section of the Report.

4.2 TRAFFIC MANAGEMENT SIGNAGE

The Main Contractor shall undertake consultation with the relevant Authorities for the purpose of identifying and agreeing signage requirements. Such signage shall be installed prior to works commencing on site. Proposed signage may include warning signs to provide warning to road users of the works access / egress locations and the presence of construction traffic. All signage shall be provided in accordance with the following Department of Transport Temporary Traffic Management Suite:

- Traffic Signs Manual – Chapter 8 – Temporary Traffic Measures and Signs for Roadworks” Department of Transport, August 2019
- Temporary Traffic Management Design Guidance” Department of Transport, August 2019



- Temporary Traffic Management Operations Guidance” Department of Transport, August 2019 Part 0 Introduction and Background
 - Part 1 – Level 1 Roads – Urban and Low Speed Roads
 - Part 2 – Level 2 Roads – Rural Single Carriageway Road
 - Part 3 – Level 3 Roads – Dual Carriageways and Motorways

And also, “The Guidance for the Control and Management of Traffic at Roadworks (2nd Edition)” – Department of Transport, NRA and Local Government Management Services Board, October 2010

In summary, the Contractor will be required to ensure that the following elements are implemented:

- Consultation with the relevant authorities for the purpose of identifying and agreeing signage requirements.
- Provision of temporary signage indicating site access route and locations for contractors and associated suppliers; and
- Provision of general information signage to inform road users and local communities of the nature and locations of the works, including project contact details.

4.3 ROUTING OF CONSTRUCTION TRAFFIC

As outlined in Section 2.2.1, preferred construction phase access would be from the Southwestern Boundary at the proposed development access. The delivery/haulage vehicles will be routed depending on the destination of the materials being delivered. It is envisaged that all construction traffic will approach the site from the N84 direction.

The use of local roads will be minimised as much as possible, particularly to avoid / minimise the encountering of narrow road widths, poor visibility and unsuitable bearing capacities.

4.4 PROGRAMMING

To reduce impacts on local communities and residents adjacent to the proposed works site, it is proposed that:

- The Contractor will be required to liaise with the management of other construction projects and the Local Authorities to co-ordinate deliveries.
- The Contractor will be required to schedule deliveries in such a way that construction activities and deliveries activities do not run concurrently e.g., avoiding pouring of concrete on the same day as material deliveries to reduce the possibility of numbers of construction delivery vehicles arriving at the site simultaneously, resulting in build-up of traffic on road network.
- The Contractor will be required to schedule deliveries such that traffic volumes on the surrounding road network is kept to a minimum.



- A construction phase programme of works shall be developed by the Contractor in liaison with Galway City Council specifically considering potential road repair works that are included in the Local Authority's Road works schedule. Works should be programmed where possible such that any road works are carried out following the presence of construction traffic for the proposed development.
- HGV deliveries to the development site will be suspended on the days of any major local events, school events (graduations, etc), sports events, etc. that have the potential to cause larger than normal traffic volumes.
- The Contractor will be required to interact with members of the local community to ensure that deliveries will not conflict with sensitive events such as funerals.
- The Contractor will coordinate with the local school's management so that HGV deliveries will be scheduled to avoid clashing with the opening, lunchtime and closing times of the school where it is reasonably practicable. This is to minimise the impact of same on the everyday school traffic i.e., school buses, children being dropped off / collected by parents, etc. This is to also ensure that any interaction between HGVs and children / staff is minimised or eliminated altogether.
- Site development and building works shall be carried out only between the hours of 08.00 to 19.00 Mondays to Fridays inclusive and between 08.00 to 14.00 on Saturdays and not at all on Sundays and public holidays (or as directed by condition of Planning). Deviation from these times will only be allowed in exceptional circumstances where prior written approval has been received from the planning authority.

4.5 RECOMMENDED TRAFFIC MANAGEMENT SPEED LIMITS

Adherence to posted / legal speed limits will be emphasised to all staff / suppliers and contractors during induction training. Drivers of construction vehicles / HGVs will be advised that vehicular movements in sensitive locations, such as local community areas, shall be restricted to 50 km/h. Special speed limits of 30 km/h shall be implemented for construction traffic in sensitive areas such as adjacent to and within the construction site. Such recommended speed limits will only apply to construction traffic and shall not apply to general traffic. It is not proposed to signpost such speed limits in the interest of clarity for local road users.

4.6 ROAD CLEANING

It shall be a requirement of the works contract that the Main Contractor will be required to carry out road sweeping operations to remove any project related dirt and material deposited on the road network by construction/delivery vehicles. Road Sweepers will dispose of material following sweeping of road network, to licensed waste facility.

4.7 VEHICLE CLEANING

It shall be a requirement of the works contract that the Main Contractor will be required to provide wheel washing and/or other means necessary to remove mud and organic material from vehicles exiting the site.



4.8 ROAD CONDITION

The extent of the heavy vehicle traffic movements and the nature of the payload may create problems of:

- Fugitive losses from wheels, trailers or tailgates; and
- Localised areas of subgrade and wearing surface failure on Bothar an Chóiste.

The Contractors shall ensure that:

- Loads of materials leaving each site will be evaluated and covered if considered necessary to minimise potential dust impacts during transportation.
- The transportation Contractor shall take all reasonable measures while transporting waste or any other materials likely to cause fugitive losses from a vehicle during transportation to and from site, including but not limited to:
 - Covering of all waste or material with suitably secured tarpaulin/ covers to prevent loss; and
 - Utilisation of enclosed units to prevent loss.
- The roads forming part of the haul routes will be monitored visually throughout the construction period and a truck mounted vacuum mechanical sweeper will be assigned to roads along the haul route as required.
- Throughout the course of the construction of the proposed development, ongoing visual inspections and monitoring of the Bothar an Chóiste will be undertaken to ensure any damage caused by construction traffic is recorded and that the relevant Local Authority is notified.

Arrangements will be made to repair any such damage to an appropriate standard in a timely manner such that any disruption is minimised. Upon completion of the construction of the proposed development, the surveys carried out at preconstruction phase shall be repeated and a comparison of the pre and post construction surveys carried out. Where such comparative assessments identify a section of Bothar an Chóiste as having been damaged or as having deteriorated because of construction traffic, the road will be repaired to the preconstruction standard or better.

4.9 ROAD CLOSURES

During the works, it is not envisaged that road closures will be required. In areas where existing carriageways are narrow, it is envisaged that Traffic Management measures such as temporary traffic lights will be utilised to facilitate traffic. It is envisaged however that temporary road closures will be required at guarding locations for the purpose of removal following construction. These closures will be short in duration, with road closure times and appropriate measures to be agreed with Galway City Council and other relevant stakeholders prior to the removal of guarding.



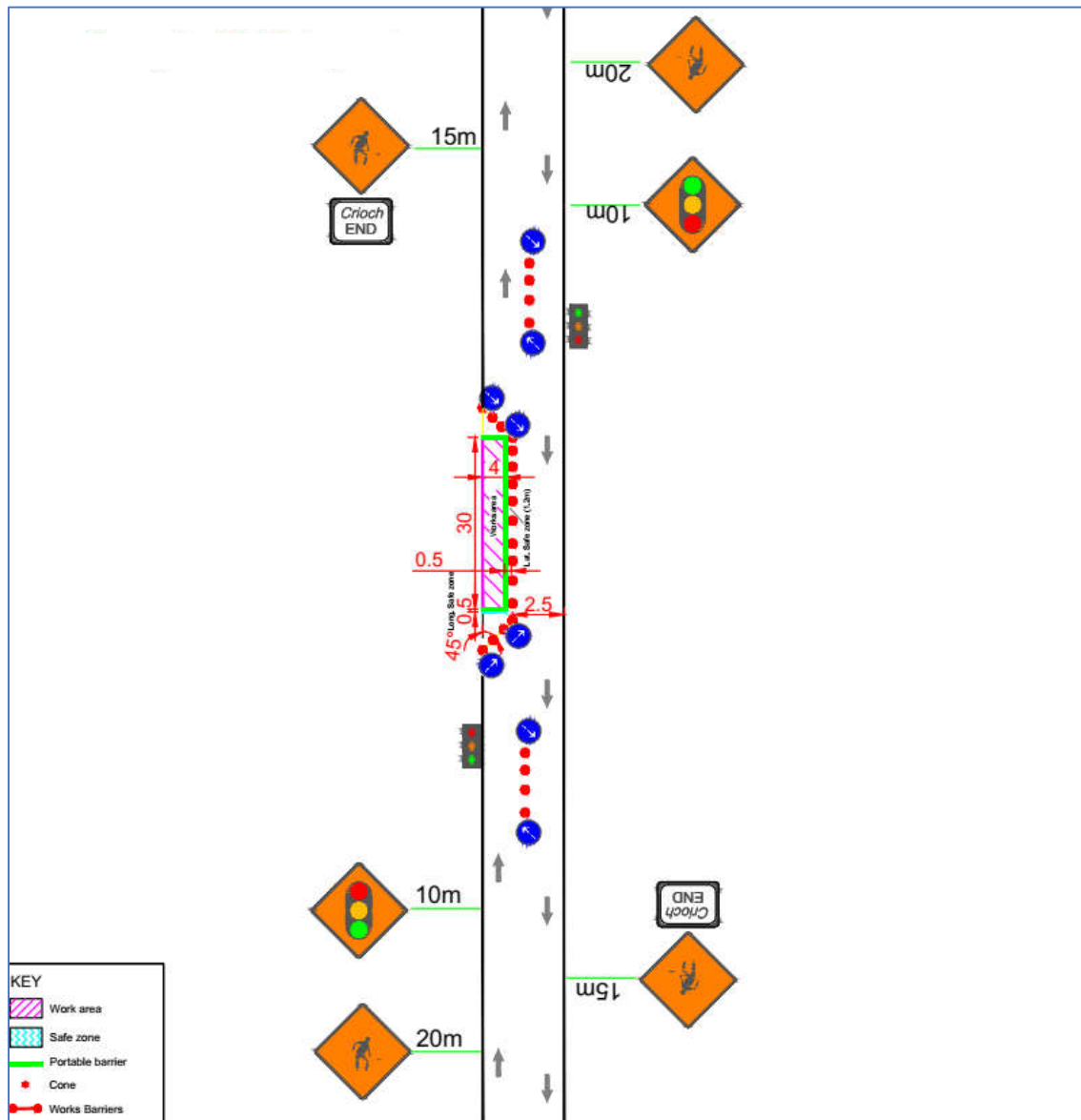


Figure 4-1: Example of Temporary Traffic Management Set-up

4.10 ENFORCEMENT OF TRAFFIC MANAGEMENT PLAN

All project staff and material suppliers will be required to adhere to the final TMP. As outlined above, the principal contractor shall agree and implement monitoring measures to confirm the effectiveness of the TMP. Regular inspections / spot checks will also be carried out to ensure that all project staff and material supplies follow the agreed measures adopted in the TMP.

4.11 DETAILS OF WORKING HOURS AND DAYS

Site development and building works shall be carried out only between the hours of 08.00 to 19.00 Mondays to Fridays inclusive and between 08.00 to 14.00 on Saturdays and not at all on Sundays and public holidays (or as directed by condition of Planning). Deviation from these times will only be allowed in exceptional circumstances where prior written approval has been received from the Planning Authority.

4.12 EMERGENCY PROCEDURES DURING CONSTRUCTION

The Contractor shall ensure that unobstructed access is provided to all emergency vehicles along all routes and site accesses. The Contractor shall provide to the local authorities and emergency services, contact details of the Contractor's personnel responsible for construction traffic management. In the case of an emergency the following procedure shall be followed:

- Emergency Services will be contacted immediately by dialling 112.
- Exact details of the emergency / incident will be given by the caller to the emergency line operator to allow them to assess the situation and respond in an adequate manner.
- The emergency will then be reported to the Site Team Supervisors and the Safety Officer.
- All construction traffic shall be notified of the incident (where such occurs off site).
- Where required, appointed site first aiders will attend the emergency immediately; and
- The Safety Officer will ensure that the emergency services are en-route.

4.13 COMMUNICATION

The Contractor shall ensure that close communication with Galway City Council and the emergency services shall be maintained throughout the construction phase. Such communications shall include:

- Submissions of proposed traffic management measures for comment and approval.
- Ongoing reporting relating to the condition of the road network and updates to construction programming; and
- Information relating to local and community events that could conflict with proposed traffic management measures and construction traffic to implement alternative measures to avoid such conflicts.

The Contractor shall also ensure that the local community is informed of proposed traffic management measures in advance of their implementation. Such information shall be disseminated by posting advertisements in local newspapers and delivering leaflets to houses in the affected areas. Such information shall contain contact information for members of the public to obtain additional information and to provide additional knowledge such as local events, sports fixtures etc. which may conflict with proposed traffic management measures.



5.0 CONCLUSION

The preliminary Construction Traffic Management Plan (TMP) will form part of the construction contract and will be designed to reduce possible impacts which may occur during the construction of the proposed development. This preliminary CTMP shall be used by the appointed Contractor as a basis for the preparation of a final CTMP and shall detail, at a minimum, the items detailed in this preliminary CTMP and any subsequent requirements of the local authorities.

The Employer shall be responsible for ensuring that the Contractor manages the construction activities in accordance with this preliminary CTMP and shall ensure that any conditions of planning are incorporated into the final CTMP prepared by the appointed works Contractor.

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